



LARAMIE COUNTY PLANNING & DEVELOPMENT DEPARTMENT

Planning • Building

MEMORANDUM

TO: Laramie County Board of Commissioners

FROM: Cate Cundall, Associate Planner

DATE: May 5, 2026

TITLE: PUBLIC HEARING regarding a Vacate of Lots 1 and 2, Block 1, Afflerbach Addition, and approval of a Modest Plat for Afflerbach Addition, 2nd Filing, Laramie County, WY.

EXECUTIVE SUMMARY

Steil Surveying Services, LLC, on behalf of Laramie County School District #1, has submitted a Modest Plat application for Afflerbach Addition, 2nd Filing for a replat of Lots 1 and 2, Block 1, Afflerbach Addition. The subject property is located at 400 W. Wallick Road, Cheyenne, WY. The purpose of the application is to combine both lots into one 49.33 acre tract that will be the site of the new 5-6 Elementary School.

BACKGROUND

The subject property is currently assessed as exempt land located in a Public (P) zone district and where Afflerbach Elementary School is located. The surrounding area is used for residential. The Afflerbach Addition Plat was approved on June 23, 1986, and the BOCC must approve the vacation of that plat for this Modest Plat to be approved.

Pertinent Statutes and 2025 Laramie County Land Use Regulations Include:

Wyoming Statutes § 18-5-304 and 34-12-106.

Section 2-4-109 governing the P – Public Zone District.

Section 4-2-103 governing Modest Plats with lot line adjustments within recorded subdivisions.

DISCUSSION

The Laramie County Comprehensive Plan identifies the area as Urban Rural Interface (URI) that is intended to accommodate a mix of more intensive land uses than other areas. The parcel lies in the PlanCheyenne Public (PUB) district.

South Cheyenne Water and Sewer District provides water and sanitary services to the property.

Agency review comments have been addressed. Public notice was provided per section 1-3-100 of the 2025 LCLUR. No comments were received.

RECOMMENDATION and FINDINGS

Based on evidence provided, staff finds that:

- a. This applications meets the criteria for a modest plat with lot line adjustments within recorded subdivisions pursuant to Section 4-2-103 of the 2025 Laramie County Land Use Regulations.
- b. This application is in conformance with section 2-4-109 governing the P - Public zone district.

and the Board of County Commissioners may approve the Vacation of Lots 1 and 2, Block 1, Afflerbach Addition to become effective upon recordation of the Modest Plat known as Afflerbach Addition, 2nd Filing with one condition and to adopt the finding of facts a and b of the staff report.

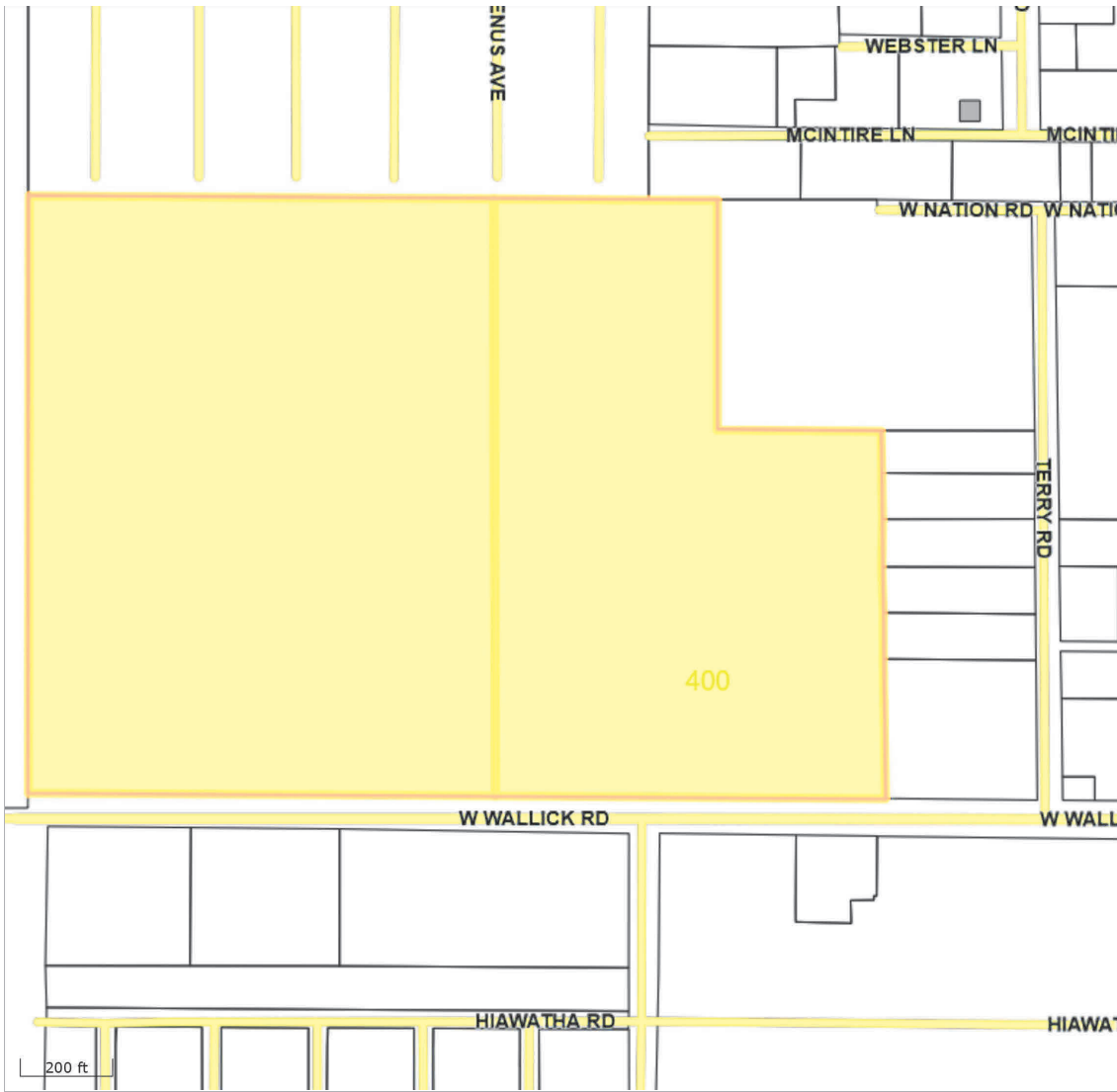
1. **The Vacation of Lots 1 and 2, Block 1, Afflerbach Addition will become effective upon the recordation of Afflerbach Addition, 2nd Filing.**

PROPOSED MOTION

I move to approve the Vacation of Lots 1 and 2, Block 1, Afflerbach Addition, and approve the Modest Plat for Afflerbach Addition, 2nd Filing with one condition and adopt the findings of facts a and b of the staff report.

ATTACHMENTS

- Attachment 1: Location Map**
- Attachment 2: Project Narrative**
- Attachment 3: Pre-Application Meeting Notes**
- Attachment 4: CFF & PSF Acknowledgement**
- Attachment 5: Traffic Impact Study**
- Attachment 4: Combined Agency Comments and Applicant Response**
- Attachment 5: Resolution**
- Attachment 6: Modest Plat Rev. 4.9.26**



Afflerbach Addition,
2nd Filing Modest Plat

PZ-26-00024

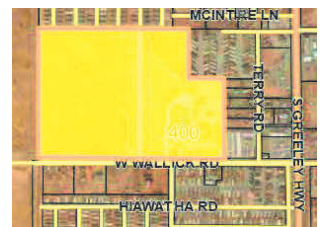
400 West Wallick Road
Cheyenne, Wyoming

Public Zone District

Fire District Number 1

South Cheyenne Water
& Sewer District

Laramie County School
District Number One





March 18, 2026

Laramie County Planning & Development Office
3966 Archer Parkway
Cheyenne, WY 82007
(307) 633-4303

InRe: LETTER OF JUSTIFICATION – Afflerbach Addition Block 1, Lots 1 and 2 replat, situate in a portion of Section 17, Township 13 North, Range 66 West, 6th P.M., Laramie County Wyoming.

Steil Surveying Services, agent for the owner, intends to subdivide the above referenced property to combine Lots 1 and 2, Block 1, Afflerbach Addition. tracts. A zone change is not required.

Please contact me with any questions or concerns.

Sincerely,

A handwritten signature in blue ink that reads "Michael S. Hansen".

Shane Hansen

Director Planning and Development
Steil Surveying Services, LLC
shansen@steilsurvey.com



Laramie County, WY
Laramie County Planning and Development Office

3966 Archer Pkwy
Cheyenne, WY 82009
(307) 633-4303
www.laramiecountywy.gov
planning@laramiecountywy.gov

PERMIT

PA-25-00071

PRE-APPLICATION MEETINGS

SITE ADDRESS: UNKNOWN LARAMIE COUNTY
PRIMARY PARCEL: 13661730501400
PROJECT NAME: GRADE 5 & 6 TRIAD SCHOOL

ISSUED: 09/03/2025
EXPIRES: 03/02/2026

APPLICANT: LARAMIE COUNTY SCHOOL DIST NO 1
2810 HOUSE AVE
CHEYENNE, WY 82001
307-771-2248

OWNER: UNKNOWN CONTACT UNKNOWN
123 XXX
XX, XX 00000

ADDITIONAL PROPERTY OWNER: LEGACY UNKNOWN
123 XXX XX, XX. 00000

ARCHITECT: Jessie Dykhouse
237 N MAIN ST STE 110
SHERIDAN, WY 82801

Detail Name	Detail Value
Meeting Date	09/03/2025
MEETING AM OR PM	PM
Application Types	Site Plan
Attendees	In Person (3966 Archer Pkwy)
Property Interest	Owner
Detailed Project Narrative	New elementary school
Staff Attending	Justin, Sonny, Cate, Dan, Scott S., Cadi,
Application Fees	Yes
Copy of Pre-App Notes	REQUIRED FOR APPLICATION SUBMITTAL
Project Narrative Letter	Yes
Warranty Deed and/or Lease Agreement	Yes
Development Action	Site Plan
Drainage Plans	Yes
Drainage Study	Yes
Traffic Study	Yes
Public Safety Fees Acknowledgement Letter	No
Community Facility Fees Acknowledgement Letter	No
WY DEQ Chapter 23 Study/Submittal Letter	No



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Development Agreement	No
Roadway Maintenance Plan	TBD
Road/Easement Use Agreement	TBD
Right-of-Way Construction Permit	Upon Construction
Engineer Review - Paid by Applicant	Yes
Environmental Health Review/Approval	Yes
Environmental and Services Impact Report	No
GESC Permit	Yes, Standard
Floodplain Development Permit	No
Perimeter Fence Construction per W.S.S. 18-5-319	No
Public Notice, Paid by Applicant	Yes
Newspaper Legal Notice, Paid by Applicant	No
Adjacent Property Owner Letter, Paid by Applicant	Yes
Miscellaneous Notes	Scope of the project is construction of a new school on vacant land. Access will be off Wallick Drive. Located on western parcel, utilities on eastern side. Nested in SW corner connecting to SCWSD on western parcel. DEQ permits will be obtained. Wants to integrate design plan of Wallick Road into their project. Scoping study is still being considered. 650 students anticipated, 92,000 square foot building. Bus loop will be off Wallick until Division is developed. Division part north will need to be discussed to see how to accomplish. Retention SE corner. Two west bound lanes for access to site with additional west bound lane into facility. Wallick is proposed 3 lane. Need to know for traffic study if they can get preliminary design. Discussion on roads will include WYDOT, MPO, County. Similar to what was held for ARP.



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Miscellaneous Notes (2)

Molly Cook said the Wallick Project is being revised and is going from College to Angi Street. Division will not be included at this time. Intersection at Wallick and Division will be a hammerhead. Project anticipates three accesses. Is it possible for division be extended to bus loop? There aren't any funds at this time. Possibly school can fund? Water on Wallick and western property line. Existing sewer on parcel. Scott with South Cheyenne would like a discussion on modeling for existing load. Demand sheets will need to be prepared, probably within in a week.

Miscellaneous Notes (3)

Planning requirements. Zone district is correct. New 2025 Regulations adopted and site plan will be required. Public notification includes signs, letters. Agency reviews will be scheduled. Approval will be done administratively. Environmental Health will review. When all agency comments are completed and addressed a Certificate of Review will be issued. These are required to pull building permits. A grading permit is required with a WYPDES permit from DEQ as of the application documents. As for parking either use School or County, whatever is stricter. When building is built final Certificate of Compliance is issued. LC Building will do fire review. Grading permit is \$500, Site Plan is \$500 and actual costs of mailing, 1 sign, and engineer review fees. Submission of application is done online.

CONDITIONS

* Disclaimer: These are intended as guidance only. Fee calculations are determined at the time of application, and issues that arise during review periods are not always anticipated at pre-application stage. Public Records Act: This document and any documents provided by the applicant to Planning may constitute a public record under W.S.S. 16-4-201 et seq. Applicants are advised not to divulge any information at a Pre-Application Meeting with Planning that they do not yet desire to be public information.



Laramie County, WY

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* A traffic study may be required for any site plan, subdivision permit, or access request for any development and shall be required for any project or development that will generate 100 or more trips during any hour or over 200 trips per day. Traffic studies shall be prepared by a qualified civil engineer licensed by the Wyoming State Board of Registration for Professional Engineers and Professional Land Surveyors to practice engineering in Wyoming. The applicant and the engineer shall meet with the County prior to preparation of the traffic study to discuss specific issues or concerns. The Director of Planning and Development may waive a traffic study based on estimated ADT, and peak hour trips, or existing road or site conditions, including adequate pedestrian access.

* Requests for waivers for drainage impact studies shall be made in writing to the Laramie County Public Works Department. The County shall review the request and approve the grant for a waiver or identify the level of study required for the proposed development action. Laramie County Public Works may waive the requirement for drainage study based on the following: a. Information is provided to substantiate there are no potential drainage problems at the site or downstream of the site (including impacts to downstream floodplains). b. The development or redevelopment will not result in an increase in the historic impervious area. c. The development or redevelopment of an area is immediately adjacent to a major drainageway that is capable of conveying the fully developed basin 100-year flood without impact to the base flood elevation. d. The development or redevelopment is unlikely to create drainage problems.

* A waiver or alternative to the required landscaping may be presented to the Planning and Development Director for review. The Director shall approve the proposed alternative landscape plan based on the following criteria: A. the proposed alternative meets or exceeds the intent of this regulation, and B. the proposed alternative is well-integrated with the surrounding landscaping and land uses, and C. the proposed alternative meets the goals of Laramie County Comprehensive Plan and; D. the purpose of the required site plan is to legalize an existing use and the impact or benefits of the landscape plan on the property would be minimal; or E. the landscaping as required would prohibit reasonable use of the property.



March 18, 2026

Laramie County Planning & Development Office
3966 Archer Parkway
Cheyenne, WY 82009
(307) 633-4303

InRe: Afflerbach Addition Block 1, Lots 1 and 2 replat, situate in a portion of Section 17, Township 13 North, Range 66 West, 6th P.M., Laramie County Wyoming.

To whom it may concern:

Steil Surveying Services, agent for the owner(s), provides this letter on their behalf, confirming that the owner is exempt to the Community Facility Fee(s)(CFFs) and exempt to the Public Safety Fee(s) (PSFs) pursuant to §§1-1-106 of the Laramie County Land Use Regulations.

Sincerely,

A handwritten signature in blue ink that reads "Michael S. Hansen".

Shane Hansen

Director Planning and Development
Steil Surveying Services, LLC
shansen@steilsurvey.com

TRAFFIC IMPACT STUDY

**Proposed Cheyenne 5/6 School
402 W Wallick Road**

Laramie County, Wyoming

February 4, 2026

20250024

Prepared by:



Arvada, CO 80003
(303) 653-9200

This report has been prepared by the staff of CivTrans Engineering, Inc. on behalf of the Laramie County School District and Martin/Martin Wyoming under the direction of the undersigned professional engineer whose seal and signature appears hereon.

DRAFT

Craig A. MacPhee, PE, PTOE

Statement of Engineering Qualifications

Craig A. MacPhee, PE, PTOE is a civil and traffic engineer for CivTrans Engineering Inc. Mr. MacPhee has over twenty years of extensive traffic and transportation engineering experience. He has completed numerous transportation studies, roadway design project and traffic signal design throughout the western United States, including Wyoming, during his career. He became a licensed professional engineer in 2004 in Washington state and obtained his Wyoming professional engineer license in 2022. He obtained his professional traffic operations engineer (PTOE) certification in 2021.

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Raw Traffic Counts

Trip Generation Calculations

Crash Data

Level of Service Calculations – Existing; Short-term NO Project; Short-term with Project; Long-range NO Project; Long-range with Project

EXECUTIVE SUMMARY

This traffic impact study (TIS) document has been prepared as part of the development process for a proposed school to accommodate 5th and 6th graders (5/6) in unincorporated Laramie County, Wyoming. The following is a summary of the traffic information and findings included in this report.

1. Laramie County School District is proposing to construct a new school along the north side of Wallick Road, immediately west of Afflerbach Elementary School (west of US 85) to accommodate 5th and 6th grade students in the south triad of the school district within unincorporated Laramie County, Wyoming. The school will have a capacity of 650 students and will operate with an initial enrollment of 632 students. The district typically operates their schools at 90% of the capacity, which would be 585 students for this school. A vicinity map is included as **Exhibit 1** and the current site plan is shown on **Exhibit 2**.
2. The site is proposed to provide three accesses to Wallick Road, which will include one ingress-only driveway and one egress-only driveway for accessing the student drop-off and general parking area. A third driveway on the west side of the school will provide access for school buses.
3. The proposed 5/6 school is estimated to generate up to 1,412 daily vehicle trips with 515 occurring during the arrival (AM) peak hour and 225 during the dismissal (PM) hour trips. However, approximately 90 of the students that will attend this school currently attend Afflerbach Elementary and already travel to this area. Therefore, the net increase in trips to Wallick Road is estimated to be approximately 1,222 daily trips with 448 during the AM peak hour and 195 during the dismissal hour.
4. The study area was identified to include the followings intersections.
 - Avenue B-2 & Wallick Road
 - S. Greeley Highway (US 85) & Wallick Road
 - S. Greeley Highway (US 85) & College Drive

These intersections were analyzed for the weekday AM and dismissal peak hours.

5. The analysis horizons considered and evaluated in this report include:
 - Existing Conditions (Year 2025)
 - Short-term without Project Conditions (Year 2029)
 - Short-term with Project Conditions (Year 2029)
 - Long-range No Project Conditions (Year 2050)
 - Long-range with Project Conditions (Year 2050)

Each of these analysis horizons include intersection capacity analysis, speed change lane and sight distance evaluations. The existing condition also includes an intersection safety (crash) evaluation. The future condition with the project

includes safe routes to school and school speed zones discussions. A traffic signal warrant evaluation was conducted for the intersection of US 85 & Wallick Road to determine when a traffic signal should be installed at that intersection.

6. Laramie County has established a minimum level of service D (LOS D) for acceptable operations at signalized intersections and unsignalized (stop controlled) approaches.

Existing Conditions

7. The existing intersections within the study area are currently operating at acceptable levels except the intersection of S Greeley Highway & Wallick Road, which is operating at LOS E during the dismissal hour. A traffic signal warrant evaluation utilizing the Manual on Uniform Traffic Control Devices (MUTCD) was conducted for this intersection for the existing conditions. The intersection does not meet warrants for a traffic signal at this time.
8. The posted speed on Wallick Road includes a 20-mph school zone for westbound travel from 6:30 AM to 5:00 PM, but nothing for eastbound travel. The duration of the school zone limitation seems excessive unless the school is commonly utilized by children outside of school hour for playground purposes. Laramie County should review the posted speed limit signage along this roadway and install appropriate speed limit signs for both directions.
9. Based on the Laramie County Land Use Regulations criteria for speed change lanes, the intersection of Avenue B-2 & Wallick Road has turning volumes that exceed the 10 vph thresholds for a westbound left turn deceleration lane. A left turn lane should be constructed at this intersection. This is included in the Wallick Road improvement plans, which will widen the roadway to a 3-lane section from S Greeley Highway to Division Avenue.
10. Crash data for the study area intersections was collected for the most recent five years. None of the study area intersections is estimated to have a crash frequency greater than 1.0 crashes per million entering vehicles (MEV), but the intersection of S Greeley Highway & College Drive has a rate of 0.94 crashes per MEV and had a fatality in May 2024. The fatality was a result of a vehicle failing to yield to oncoming traffic, making a left turn from eastbound College Drive. The crash rate near 1.0 and a fatality occurring at this intersection may warrant further investigation for suitable safety improvements at the intersection.
11. Entering sight distance (triangle) criteria is listed in Table 5-6-3 within the Laramie County Land Use Regulations and section 6.2 of the WYDOT Traffic Studies Manual. The required sight distance is based on the posted speed of the major roadway, number of travel lanes and the design vehicle. For the purposes of this analysis, it is assumed that each intersection must accommodate a single-unit truck over 10,000 lb gross vehicle weight (GVW). Based on a field visit, each of the stop-controlled intersections appears to have adequate entering sight distance.

12. Traffic observations were conducted during the dismissal hour of Afflerbach Elementary on December 18, 2025 by CivTrans Engineering staff. Blockage of eastbound Wallick Road occurs for a couple of minutes as left turning vehicles from Wallick Road wait for the onsite queue to clear. The school does not have sufficient on-site storage for the pick-up operation and many vehicles park along the north and south side of Wallick Road. Wallick Road is posted with “no parking” signs along the north side of the roadway. It is not clear why the north side of Wallick Road is posted with “no parking” signs as there appears to be adequate road width to accommodate the parked vehicles and provide a westbound travel lane. Laramie County should revisit the parking prohibition on this roadway and either remove the signs or modify them to permit parking during school loading times. The Wallick Road widening project should provide off-site queuing or parking for the westbound direction for Afflerbach Elementary. The addition of an eastbound left turn lane, which is anticipated to be completed as a part of the Wallick Road widening project, should alleviate the blockage issue that currently exists.
13. A 0.5% annual growth rate was applied to South Greeley Highway, Avenue B-2 and Wallick Road to estimate future traffic volumes. A 2.0% annual growth rate was utilized for College Drive. These growth rates are based on average daily traffic volumes for 2023 and 2050 provided by the Cheyenne Metropolitan Planning Organization (MPO). The 2050 model assumes significant roadway infrastructure in the vicinity of the site is completed, which will likely be tied to development along these corridors. Significant to this study area are the extension of Wallick Road approximately two (2) miles to the west and to the east through the Sweetgrass development, Division Avenue connecting from College Drive to High Plains Road and beyond, and Avenue C connecting from its existing terminus at Murray Road through High Plains Road and beyond. Therefore, trips associated with the Wallick Road extension to the east were added in addition to the ambient growth for all of the roadways.

Short-term without the Project

14. The short-term condition without the project is anticipated to operate similar to existing conditions without significant degradation to the level of service at the study area intersections.

Long-range without the Project

15. With the significant anticipated growth in traffic within the study area over the next 25 years, the added traffic from the extension of Wallick Road, Division Avenue and development in the vicinity, the intersection of S. Greeley Highway & Wallick Road is anticipated to meet warrants for signalization. Additionally, the intersection of S Greeley Highway & College drive is shown to have approaches operating at LOS F for this scenario, which are anticipated to be able to be addressed with signal timing modifications.

Short-term with Project

16. With the addition of traffic generated by the proposed project, all of the intersections within the study area are anticipated to continue to operate within acceptable levels except the eastbound approach of S. Greeley Highway & Wallick Road, which is anticipated to fall to LOS F during AM peak and dismissal hours with the project traffic added.

A traffic signal warrant evaluation was conducted for the intersection of S. Greeley Highway & Wallick Road for the short-term with project condition. The intersection is anticipated to meet Warrant 2 (four-hour) and Warrant 3 (peak hour) are anticipated to be met with the added traffic from the proposed school. With two of the MUTCD volume warrants for signalization anticipated to be met once the school is constructed and operational, a traffic signal should be considered for installation at this intersection. Installation of a traffic signal is anticipated to alleviate delay and level of service deficiencies at the intersection.

17. The added school traffic will meet left turn and right turn deceleration lane thresholds along Wallick Road at the ingress-only access to the parking and drop-off/pick-up area. Therefore, these speed change lanes should be installed at this intersection as a part of the project.

Since the egress-only access will be a tee intersection, the school should also consider providing a left turn acceleration lane on Wallick Road at this driveway to provide a safer left turn maneuver for the future once Wallick Road is extended west and carrying higher traffic volume.

18. The plans for the Cheyenne 5/6 School show three new driveways to Wallick Road. Wallick Road does not have a posted speed limit aside from the school zone associated with Afflerbach Elementary. The 20-mph school zone should be extended to accommodate the new school. The required sight distance along a 20 mph 2-lane roadway is 200' for passenger cars and pickup trucks. These lines of sight should be shown on the construction plans. The resulting sight triangle shall be kept free from obstructions to vision between the heights of 2.5' and 12' above the street grade.

19. The Cheyenne MPO Safe Routes to School Plan provides recommendations for pedestrian and bicycle infrastructure improvements for the Afflerbach Elementary School. Where achievable, the recommended improvements should be completed to provide safe walking and bicycle infrastructure for non-motorist travelers.

Long-range with Project

20. The study area intersections are mostly anticipated to operate at levels shown in the no-project conditions. The project-related recommendations included in the short-term conditions are anticipated to be sufficient for long-range considerations.

Based upon the analysis, field observations, assumptions, methodologies and results which are provided in the body of this document, this project is anticipated to have impacts to the capacity of the study area intersections that will require mitigation.

It should be noted; this traffic study was completed in accordance with the Laramie County standards as well as standard traffic engineering practices. Results are quantitative and based on Highway Capacity Manual methodologies and Laramie County criteria for capacity of intersections and roadways. It does not, nor could it, address subjective measurements such as quality of life.

INTRODUCTION

Project Overview

Laramie County School District is proposing to construct a new school along the north side of Wallick Road, immediately west of Afflerbach Elementary School (west of US 85) to accommodate 5th and 6th grade students in the south triad of the school district within unincorporated Laramie County, Wyoming. The site is proposed to provide three accesses to Wallick Road, which will include one ingress-only driveway and one egress-only driveway for accessing the student drop-off and general parking area. A third driveway on the west side of the school will provide access for school buses.

The school will have a capacity of 650 students and will operate with an initial enrollment of 632 students. The district typically operates their schools at 90% of the capacity, which would be 585 students for this school. Approximately 90 of the students that will attend this school currently attend Afflerbach Elementary and already travel to this area. Completion of the Arp Elementary project and operation of the school is anticipated by fall 2027.

The proposed 5/6 school will be part of the South Triad of the school district and will serve all of the district's 5th and 6th graders within the south triad. The South Triad serves the southern portion of the Cheyenne metropolitan area within Laramie County, Wyoming. This includes all portions that are both east of I-25 and south of I-80 within the district as well as some areas north of I-80 that are currently served by Cole, Hebard, Fairview, Lebhart, Bain and Sunrise Elementary Schools.

The project is bordered by vacant land to the west, Afflerbach Elementary to the east and existing residential uses to the north and south. Wallick Road is unpaved along the frontage of the proposed project. South Greeley Highway (US 85) is approximately ¼ mile east of the site and provides the primary local and regional access for the area. Most of the area that will be served by this school lies to the north.

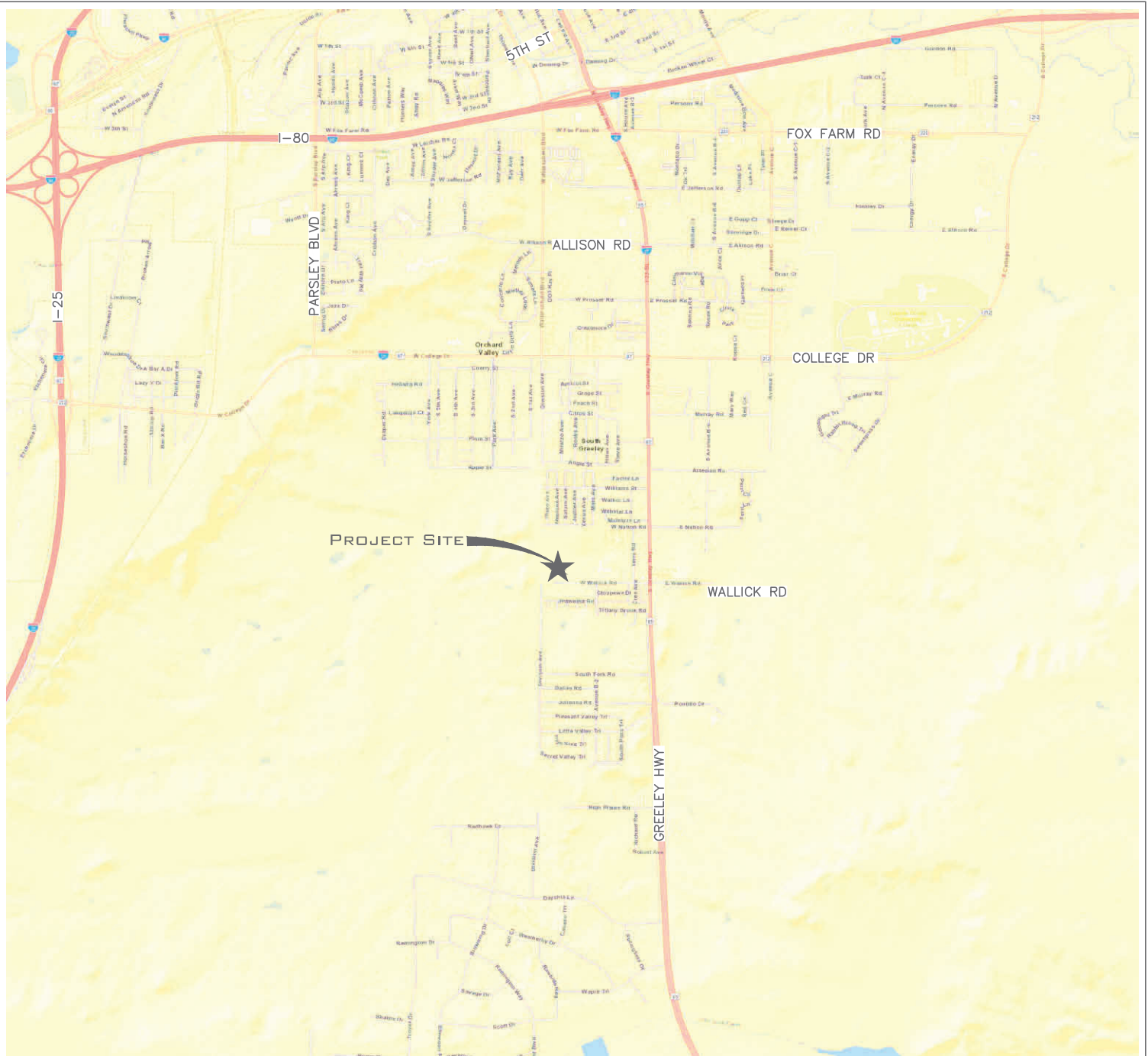
Purpose of Report

The purpose of this study is to review, assess and identify potential traffic related impacts that the proposed project may have on the transportation network and recommend mitigation to minimize these impacts where necessary and possible. **Exhibit 1** shows the general vicinity of the project in Laramie County. The current site plan is included as **Exhibit 2**, which shows the general layout of the proposed site as well as locations of parking and driveway access. A current aerial of the site and its immediate vicinity is included as **Exhibit 3**.

The assumptions utilized in conducting the traffic analysis are based on a traffic scoping meeting conducted with Laramie County, City of Cheyenne, Cheyenne Metropolitan Planning Organization (MPO), and the Wyoming Department of Transportation (WyDOT) staff. The TIS was completed in accordance with the current traffic guidelines outlined

within the Laramie County Land Use Regulations (Aug 5, 2025) as well as standard traffic engineering practices.

This report includes an evaluation and assessment of the study area for the existing conditions, short-term conditions and the long-range conditions. The short-term condition considers the transportation network shortly after the opening of the proposed project. The long-range conditions considered the potential growth in traffic within the study area and how the existing transportation system will handle those volumes with and without the proposed project approximately 25 years into the future (Year 2050). The long-range conditions also assume significant additions to the surrounding roadway network and development in the vicinity of the site, which include an extension of Division Avenue along the west side of the site, continuation of Wallick Road to the west and east, and Avenue C extending from its current terminus at Murray Road through Wallick and beyond. Weekday AM and school dismissal peak hours of traffic operations were used as the basis of this study.



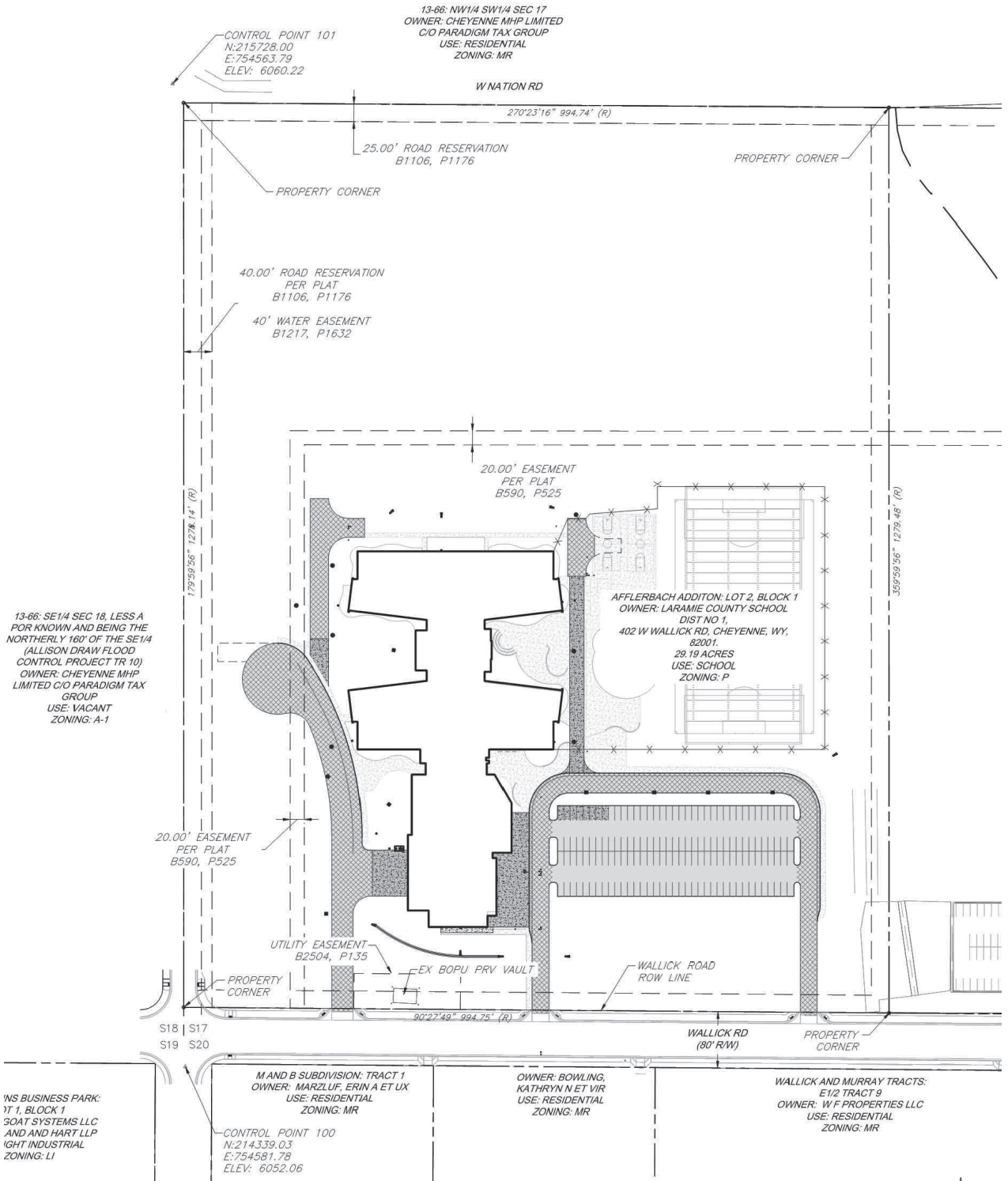
PROJECT SITE



NOT TO SCALE
 FEBRUARY 4, 2026

EXHIBIT 1
 VICINITY MAP





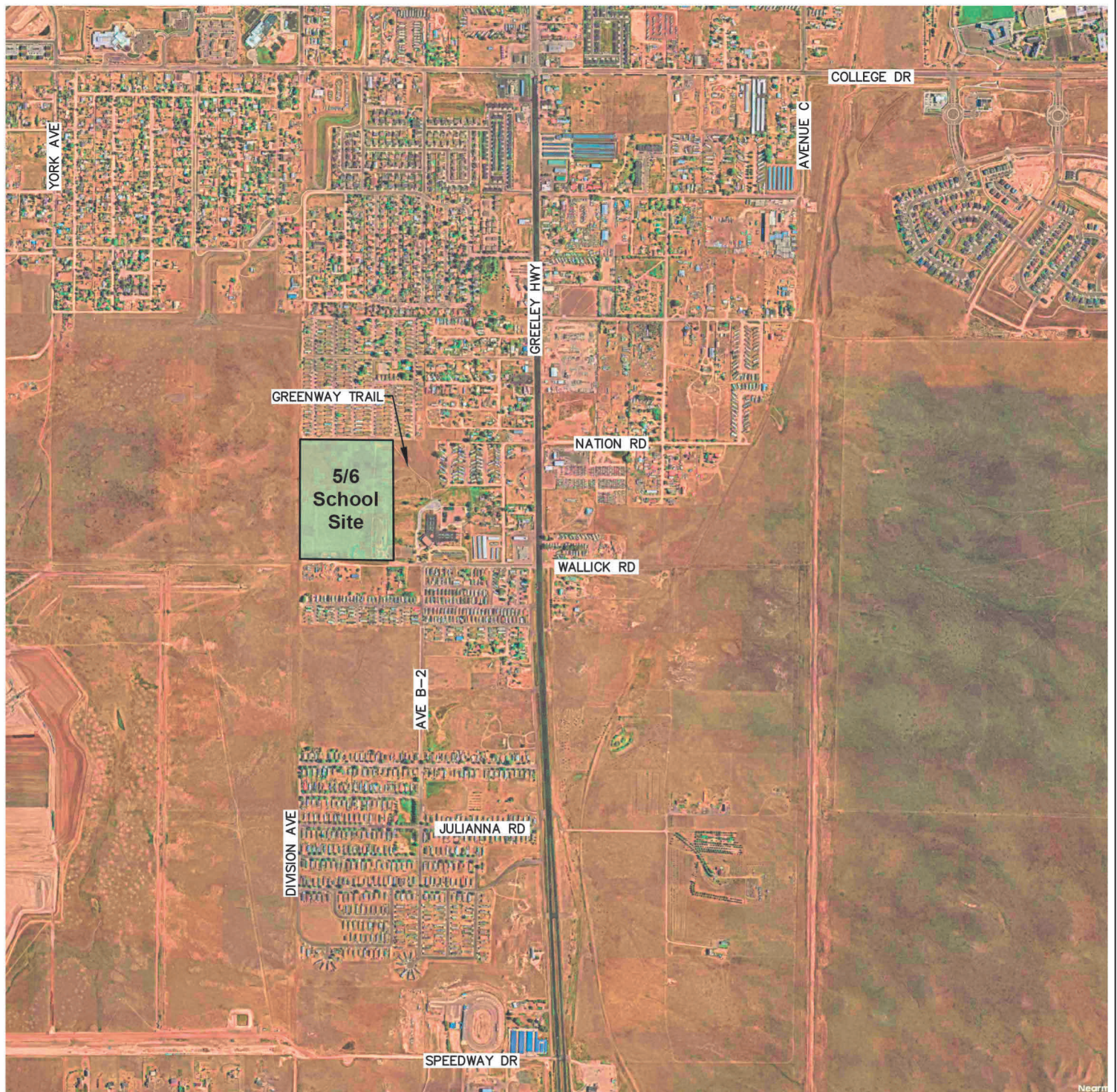
SOURCE: MARTIN/MARTIN



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FEBRUARY 4, 2026

EXHIBIT 2
CURRENT SITE PLAN





SOURCE: NEARMAP (JULY 2024)



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FEBRUARY 4, 2026

EXHIBIT 3
CURRENT AERIAL



ANALYSIS METHODOLOGY

The various analyses conducted and reported in this document include intersection capacity analysis, crash analysis, auxiliary lane evaluation and sight distance analysis.

Capacity Analysis

The analyses described in this report were performed in accordance with the procedures in the *Highway Capacity Manual* (HCM) and as described below. The analyses and procedures conducted are based upon the worst-case conditions that occur during a typical weekday. Therefore, most of each weekday and the weekends will experience traffic conditions better than those described within this document, which represent the peak hours of operation only.

Level of Service (LOS) is an empirical premise developed by the transportation profession to quantify driver perception for such elements as travel time, number of stops, total amount of stopped delay, and impediments caused by other vehicles afforded to drivers who utilize the transportation network. LOS has been defined by the Transportation Research Board in the *Highway Capacity Manual, 7th Edition*. This document has quantified level of service into a range from “A” which indicates little, if any, vehicle delay, to “F” which indicates significant vehicle delay and traffic congestion that may lead to system breakdown due to volumes that may far exceed capacity.

The *Highway Capacity Manual* defines the level of service for a signalized intersection as the average delay per vehicle (amount of time a vehicle must spend at the intersection) for the overall intersection. For unsignalized intersections that include both stop-controlled and uncontrolled approaches (known as through/stop controlled), the *Highway Capacity Manual* defines the level of service as the average delay per vehicle for the worst approach, not the overall intersection.

The level of service letter grades as defined by the Transportation Research Board and the associated amount of delay in seconds per vehicle, as well as a brief description of the operating condition, for both signalized and unsignalized intersections are included for reference in **Table 1** on the next page.

Laramie County has established level of service D as the minimum acceptable intersection operating condition. Analysis results indicating operations worse than the minimum acceptable level were considered for mitigation measures. In the cases where existing conditions currently operate at or future background conditions are projected to operate at states poorer than the minimum acceptable level, the future with project conditions will be evaluated to maintain the current or projected operating conditions. Intersection design should result in LOS C or better conditions.

Table 1 - Intersection Analysis Criteria

Signalized Intersection Level of Service Criteria

Level of Service	Delay Range (seconds/vehicle)	Expected Delay at Intersection
A	≤ 10	Very low delay. Most vehicles do not stop.
B	> 10 and ≤ 20	Generally good progression of vehicles. Slight delays.
C	> 20 and ≤ 35	Fair progression. Increased number of stopped vehicles.
D	> 35 and ≤ 55	Noticeable congestion. Large portion of vehicles stopped.
E	> 55 and ≤ 80	Poor progression. High delays and frequent cycle failure.
F	> 80	Oversaturation. Forced flow. Extensive queuing.

Unsignalized Intersection Level of Service Criteria

Level of Service	Delay Range (seconds/vehicle)	Expected Delay to Minor Street Traffic
A	≤ 10	Little or no conflicting traffic for minor street approach.
B	> 10 and ≤ 15	Minor street approach begins to notice absence of available gaps.
C	> 15 and ≤ 25	Minor street approach begins experiencing delays for available gaps.
D	> 25 and ≤ 35	Minor street approach experiences queuing due to a reduction in available gaps.
E	> 35 and ≤ 50	Extensive minor street queuing due to insufficient gaps.
F	> 50	Insufficient gaps of suitable size to allow minor street traffic demand to cross safely through a major traffic stream.

Source: Highway Capacity Manual (Transportation Research Board, 2022).

Crash Analysis

A crash analysis was conducted for the study area intersections. The most recent five years of crash data was obtained from the Cheyenne Metropolitan Planning Organization (MPO) website, which provides 10 years of crash data for the Cheyenne urban area. Generally, crashes are documented by type and severity of occurrence. Severities of accidents are divided between property damage only (PDO), injury (INJ) and fatality (F). Intersection safety related to crashes is determined based on the number of crashes per million vehicles entering the intersection. Rates higher than 1.0 typically warrant further evaluation of the safety of the intersection.

Speed Change Lane Evaluation

Laramie County Land Use Regulations (LUR) were used for determining the need for speed change lanes at study area intersections. The design speed and design hourly

turning volume are factors used to determine the need for acceleration and deceleration turn lanes.

Sight Distance

Entering sight distance (triangle) criteria is listed in Table 5-6-3 within the Laramie County Land Use Regulations and section 6.2 of the WYDOT Traffic Studies Manual. The required sight distance is based on the posted speed, number of travel lanes on the major roadway and design vehicle. The resulting line-of-sight creates a “sight triangle” where plantings or other visual obstructions within this triangle area should not be 2.5 to 12 feet above the street grades per the LUR.

Traffic Signal Warrant Evaluation

The intersection of S. Greeley Highway (US 85) & Wallick Road was evaluated for traffic signal warrants using the criteria set forth within the Manual on Uniform Traffic Control Devices (MUTCD). The MUTCD provides nine (9) separate warrants for evaluating the need for signalization at intersections based on traffic volumes, pedestrian volumes, proximity to a school, coordinated signal systems, crash experience, roadway network and proximity to an at-grade rail crossing.

Analysis Horizons

The following five scenarios were analyzed as part of this study during both the weekday AM and dismissal peak hours, with the corresponding volume and network configurations as indicated:

1. Existing Conditions

Analysis of the existing conditions at the study area intersections was based on the turning movement volumes collected in November 2025 and the intersection geometry and traffic control as observed in the field.

2. Short-term No Project Conditions

The short-term future year analysis includes the same roadway geometry as for the existing conditions. The forecast volumes were calculated by applying the anticipated ambient growth rate over the next four years.

3. Short-term with Project Conditions

The short-term “build” condition takes the short-term no-project traffic volumes and adds the trips associated with the proposed project.

4. Long-range No Project Conditions

The long-range conditions included the best available information for projecting the traffic volumes approximately 25 years into the future for the study area to the Year 2050. A long-range no project condition analysis, without the proposed land use and associated trip additions, was performed as a comparison for the condition with the project (build). It is assumed that Division Avenue is extended by Laramie

County by 2050 College Drive to High Plains Road and beyond. It also assumes Wallick Road is extended approximately two miles west of the site and east of US 85. Avenue C is anticipated to be completed south of Murray Road.

5. Long-range With Project Conditions

The long-range future year analysis included the same roadway geometry as for the long-range no-project conditions. The forecast volumes were calculated by combining the long-range no project volumes with the site-generated trips related to the proposed project.

EXISTING CONDITIONS

Existing Conditions within the Study Area

The purpose of this section is to document the existing conditions within the study area for the proposed project.

Land Use

The site is vacant zoned P (Public), which is appropriate for the proposed use. The site lies on the south portion of the Cheyenne urban area in an unincorporated portion of Laramie County. Therefore, most of Cheyenne and its population lies to the north. The project site is bordered by residential uses to the north and south, Afflerbach Elementary to the east, and vacant land to the west.

Existing Roadways

As shown on the site plan, the sites will be directly accessed from Wallick Road, which is a major collector roadway providing access for the Afflerbach Elementary School, Big Country Manufactured Home Community, VFW 4343 and a storage unit facility. Wallick Road is oriented east-west and provides a connection to South Greeley Highway (US 85) approximately ¼ mile east of the site. South Greeley Highway (US 85) is a north-south regional highway, which connects the area to Cheyenne north of the site. The following is a list of the surrounding streets, their functional classification, and general geometry.

Wallick Road is an east-west, two-lane, two-way major collector that provides access for the residential uses and an elementary school. It extends from the site to US 85 and serves a mobile home community on the east side of US 85 as well. It provides sidewalk infrastructure along the north side of the roadway and intermittently along the south side. Its only posted speed limit is a 20-mph school zone, effective from 6:30 AM to 5:00 PM on weekdays.

College Drive is mostly an east-west, three-lane, two-way principal arterial, but curves northward near the Laramie County Community College. It extends from I-25 through the southern portion of Cheyenne and continues through the eastern portion of Cheyenne to Four Mile Road, approximately 10 miles in total. It has a state highway designation of Highway 212 and is posted at 30 mph for the eastbound direction and 40 mph for westbound on the US 85 intersection approaches. It provides a sidewalk on at least one side of the roadway within a mile west of US 85 and ½ mile east of US 85.

South Greeley Highway (US 85) is a north-south, two-way, five-lane principal arterial roadway through the study area with two travel lanes in each direction and a center two-way left-turn lane. It extends from Interstate 80 to the Colorado-Wyoming border and beyond. North of I-80, the roadway continues through Cheyenne as I-180 (North Greeley Highway) before splitting into the Warren Avenue and Central Avenue couplet through

downtown Cheyenne. It is posted at 50-mph through the study area and generally does not provide sidewalk infrastructure.

Avenue B-2 is a two-way, two-lane local access roadway that runs north-south from Wallick Road to Lonesome Valley Trail a little over $\frac{3}{4}$ mile. It serves the manufactured home communities south of the school site. It provides pedestrian sidewalk along its length and is posted at 20 mph.

Study Area Intersections

The project study area intersections were identified through conversations with Laramie County, City of Cheyenne, Cheyenne MPO and WyDOT staff. The study area includes the following intersections:

- Avenue B-2 & Wallick Road
- S. Greeley Highway & Wallick Road
- S. Greeley Highway & College Drive

These intersections have been analyzed for level of service (LOS) for the weekday AM & PM (dismissal) peak hours and form the basis of this document.

Traffic Control and Descriptions

Avenue B-2 & Wallick Road is an unsignalized, stop-controlled tee intersection with Avenue B-2 stopping for free-flowing traffic on Wallick Road. Each approach has one approach lane and one receiving lane. Wallick Road forms the eastbound and westbound approaches. Avenue B-2 forms the northbound approach. There is a crosswalk with a school crossing sign and push-button-activated strobe on the east leg of the intersection.

South Greeley Highway & Wallick Road is an unsignalized, stop-controlled intersection with Wallick Road stopping for free-flowing traffic on S. Greeley Highway. Wallick forms the eastbound and westbound approaches at the intersection with one lane in each direction. S. Greeley Highway forms the northbound approach with a left turn lane (striped as a two-way left-turn) and two through lanes. Right turns are made from the outer travel lane. The southbound approach has one left turn lane (striped as a two-way left-turn), two through lanes and a right turn lane.

South Greeley Highway & College Drive is a signalized intersection with the following lane configuration. College Drive forms the eastbound and westbound approaches and S. Greeley Highway forms the northbound and southbound approaches.

Eastbound (College): one left, one through, one right turn lane

Westbound (College): one left, one through, one right turn lane

Northbound (S. Greeley Hwy): one left turn lane, one through lane, one through-right lane

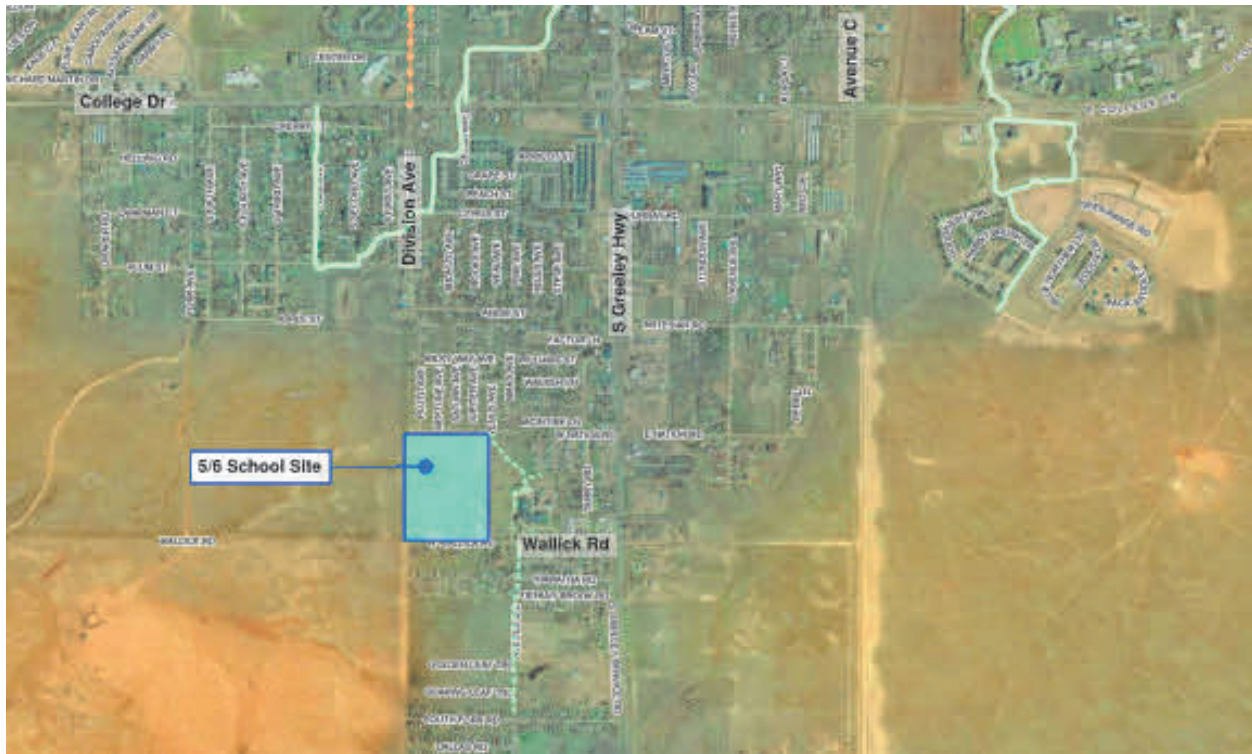
Southbound (S. Greeley Hwy): one left turn lane, one through lane, one through-right lane

All left turns at the intersection have permissive-protected phasing, which means they must yield to oncoming traffic during the flashing yellow phase. There are signalized pedestrian crossings with push buttons on all four legs. There is vehicle detection at the intersection for the signal to operate actuated.

The existing geometry at each of the study area intersections is depicted in **Exhibit 4**, which follows.

Pedestrian and Bicycle Facilities

There is limited pedestrian and bicycle infrastructure in the vicinity of the site. There is sidewalk along the north side of Wallick Road as well as shared-use trails (6-8' concrete or asphalt) connecting Afflerbach Elementary to the manufactured home communities immediately north and south of the site.



Transit

Laramie County is served with public bus by Cheyenne Transit Program (CTP). The nearest bus stop to the site is at S. Greeley Highway & Allison Road, nearly two (2) miles from the school site. The school will also operate buses for students that live more than ½ mile from the school.

Safety

Crash data for the study area intersections was obtained from the Highway Safety department at Wyoming Department of Transportation (WyDOT). The most recent five years of crash data was provided and reviewed. The provided data is shown in the table below.

Generally, crashes are documented by type and severity of occurrence. Some of the most common types of accidents include: rear-end, side-swipe, head-on, left-turn, right-angle, backing-up, lane change, and reckless driving. Severities of accidents are divided between property damage only (PDO), injury (INJ) and fatality (F).

Table 2 – Crash History

INTERSECTION	YEAR											Total	Rate per MEV*
	2020		2021		2022		2023		2024				
	PDO	INJ	PDO	INJ	PDO	INJ	PDO	INJ	PDO	INJ	F		
Avenue B-2 & Wallick Rd	0	0	0	0	0	0	0	0	0	1	0	1	0.37
S. Greeley Hwy & Wallick Rd	0	2	1	0	0	2	1	0	0	0	0	6	0.26
S. Greeley Hwy & College Dr	7	2	5	2	8	1	8	1	6	1	1	42	0.94

*MEV – million entering vehicles

Intersection safety related to crashes is determined based on the number of crashes per million vehicles entering the intersection. Rates higher than 1.0 typically warrant further evaluation of the safety of the intersection. None of the study area intersection have a rate above 1.0. There was one fatality at the intersection of S. Greeley Highway & College Drive in May 2024. The fatality was a result of a vehicle failing to yield to oncoming traffic, making a left turn from eastbound College Drive. The crash rate near 1.0 and a fatality occurring at this intersection may warrant further investigation for suitable safety improvements at the intersection.

Traffic Volumes and Peak Hours of Operation

Turning movement counts were collected by All Traffic Data Service under the direction of CivTrans Engineering staff at the study area intersections during the morning (7:00 – 9:00 AM) and afternoon (2:30 – 4:30 PM) peak periods on November 5, 2025, a Wednesday. The count at the intersection of S. Greeley Highway & Wallick Road was collected from 6:00 AM to 10:00 PM in order to perform the signal warrant evaluation. The raw count data is provided in the technical appendix. The existing traffic volumes used for intersection capacity analysis are shown in **Exhibit 5**.

Background Projects

Background projects are land development projects that have not yet been constructed but are anticipated to be completed in the near future. Therefore, their traffic would not have been included in the traffic counts collected, but should be included for future analyses. For this project, no background projects were identified for inclusion in future year traffic volumes.

Ambient Traffic Growth

Average daily traffic (ADT) volumes for 2023 and projected volumes for 2050 were supplied by Cheyenne Metropolitan Planning Organization (MPO) for the study area. The 2050 model assumes significant roadway infrastructure in the vicinity of the site is completed, which will likely be tied to development along these corridors. Significant to this study area are the extension of Wallick Road approximately two (2) miles to the west and to the east through the Sweetgrass development, Division Avenue connecting from College Drive to High Plains Road and beyond, and Avenue C connecting from its existing terminus at Murray Road through High Plains Road and beyond. As a result of these roadway connections and the associated development, Wallick Road is shown to experience significant increase in traffic volume by 2050 at approximately 4.3% annual growth. S. Greeley Highway is shown to experience a decline in traffic volumes. College Drive is shown to experience an annual growth of approximately 2%. To model future conditions within the study area, the following ambient growth rates were applied. Additionally, trips associated with the Wallick Road extension to the east were added, which are shown in **Exhibit 9**.

- Wallick Road, Avenue B-2, South Greeley Highway – 0.5% annually
- College Drive – 2.0% annually

Planned Transportation Improvements

Wallick Road is planned to be improved to provide a three-lane roadway section from South Greeley Highway to Division Avenue in the short-term. Design plans for this roadway were provided by Laramie County. No definitive timeline is known for the completion of Wallick Road west of Division Avenue, Division Avenue from College Drive to High Plains Drive and beyond, the extension of Wallick Road east of South Greeley Highway or any of the other roadways included in the 2050 Cheyenne MPO planning model. For the purposes of this report, the Wallick Road three-lane section is assumed to be completed by 2029 and the remaining roadways completed by the 2050 long-range conditions.

Peak Hour Factor

A peak hour factor (PHF) is used to convert the hourly traffic volume into a flow rate that represents the busiest 15 minutes of the peak hour. The intersection PHF derived from the traffic count was utilized for existing, short-term and long-range evaluations. However,

as traffic volumes increase in the future, the PHF is also anticipated to increase as the peak hour volume is more evenly dispersed at the study area intersections. A minimum PHF of 0.90 was assumed for long-range conditions.

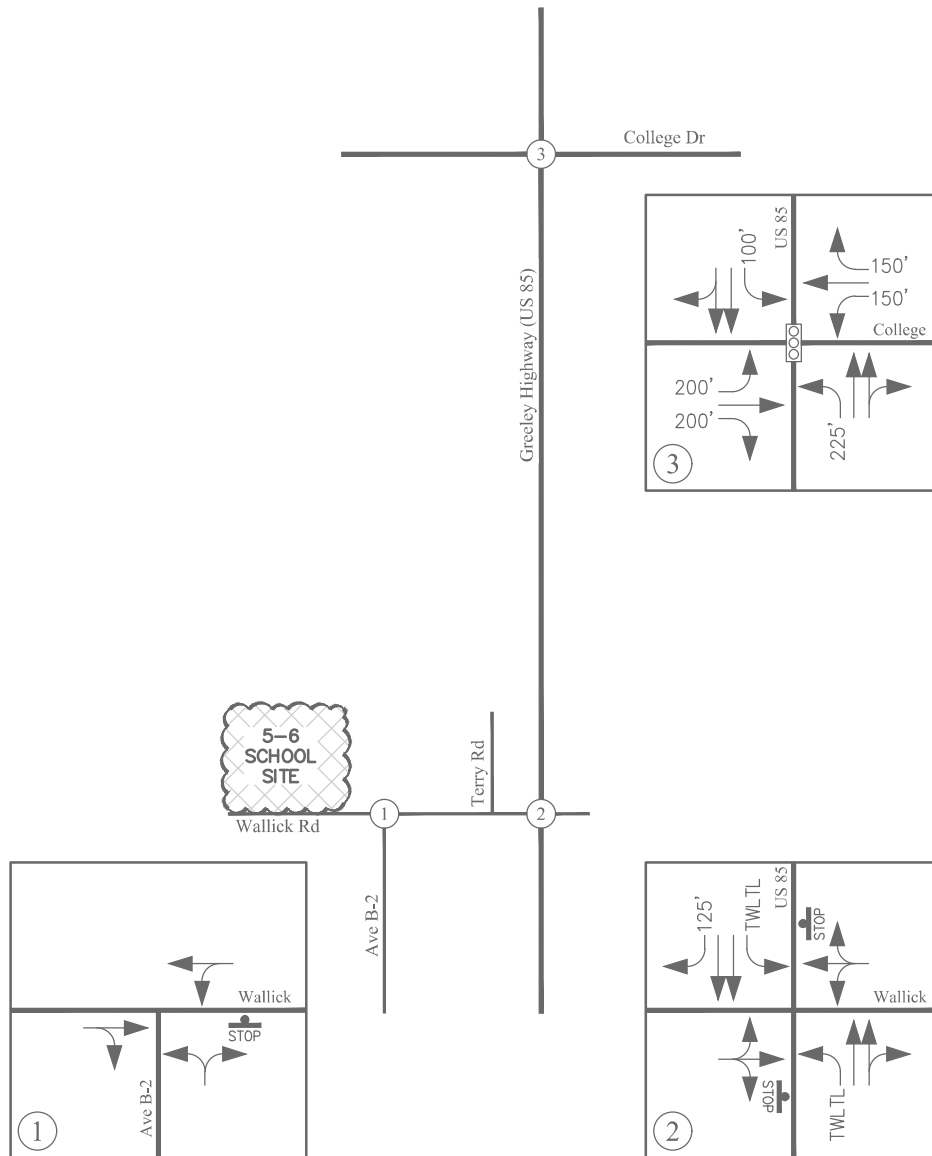
Existing Traffic Observations

Traffic observations were conducted during the dismissal hour of Afflerbach Elementary on December 18, 2025 by CivTrans Engineering staff. Overall onsite traffic operation, onsite storage utilization, queue spillback onto Wallick Road were observed and documented. The following is a summary of the observations.

School dismissal for Afflerbach Elementary is at 3:35 PM. The onsite car line for student pick-up begins to fill before 3:00 PM and several vehicle park along the north side of Wallick Road (in prohibited parking area). Parking within the angled parking lot on the site also occurs during the dismissal and a small percentage of pickups occur in the gravel lot on the west side of the school. The elementary school provides two drop-off / pick-up lanes on the school site, each providing approximately 575' of storage, which equates to 23 vehicles per lane (46 total). Moderate congestion occurs on the site and Wallick Road during the dismissal and loading period. A crossing guard is stationed at the intersection of Avenue B-2 & Wallick Road to accommodate children crossing Wallick Road west of the car line egress.

Several vehicles enter the car line as an eastbound left turn (travelling from Avenue B-2). As the car line is frequently congested and slow-moving during dismissal, these left-turning vehicles block eastbound traffic on Wallick Road. During observations, the blockage did not last longer than a couple of minutes.

It is not clear why the north side of Wallick Road is posted with "no parking" signs as there appears to be adequate road width to accommodate the parked vehicles and provide a westbound travel lane. Laramie County should revisit the parking prohibition on this roadway and either remove the signs or modify them to permit parking during school loading times. The Wallick Road widening project should provide off-site queuing or parking for the westbound direction for Afflerbach Elementary. The addition of an eastbound left turn lane, which is anticipated to be completed as a part of the Wallick Road widening project, should alleviate the blockage issue that currently exists.



— Signalized Intersection



— Stop-controlled Intersection

XXX' — Approximate storage length of turn lane without transition taper

TWLTL — Two-way left-turn lane



NOT TO SCALE
FEBRUARY 4, 2026

EXHIBIT 4

EXISTING LANE GEOMETRY



EXISTING LEVEL OF SERVICE AND TRAFFIC ANALYSIS

Capacity Analysis (Level of Service)

The existing levels of service at the subject intersections were calculated using the methods from the *Highway Capacity Manual (HCM), 7th Edition* as implemented in Synchro, *Version 12*. The existing levels of service (LOS) for the intersections within the study area are summarized on the following table. The existing traffic volumes used for this report are shown on Exhibit 5.

Table 3 -2025 Existing Intersections Levels of Service

INTERSECTION (S)ignalized (U)nsignalized		Approach Or Overall	AM Peak		Dismissal Peak	
			Delay (sec)	LOS	Delay (sec)	LOS
Avenue B-2 & Wallick Rd	U	NB	8.7	A	9.3	A
S. Greeley Hwy & Wallick Rd	U	EB	17.8	C	36.7	E
		WB	11.6	B	26.9	D
S. Greeley Hwy & College Dr	S	Ovr	23.0	C	20.9	C

According to Laramie County, a level of service (LOS) D is the minimum acceptable level for the study area intersections. Mitigation measures should be considered for overall signalized intersection or unsignalized approaches reported to be operating at LOS E or F. All of the existing study area intersections are currently operating at acceptable levels during both the AM and dismissal peak hours except the intersection of S. Greeley Highway & Wallick Road, which is currently operating at LOS E during the dismissal hour. The level of service reports for the existing conditions are provided in the Technical Appendix.

The posted speed on Wallick Road includes a 20-mph school zone for westbound travel from 6:30 AM to 5:00 PM, but nothing for eastbound travel. The duration of the school zone limitation seems excessive unless the school is commonly utilized by children outside of school hour for playground purposes. Laramie County should review the posted speed limit signage along this roadway and install appropriate speed limit signs for both directions.

Speed Change Lane Evaluation

Laramie County’s Land Use Regulations provides general criteria for speed change lanes at access points along arterials and major collectors. The following thresholds are listed in section 5-6-106-e.

- Left turn deceleration lane – greater than 10 turning vehicles per hour (vph)
- Right turn deceleration lane – greater than 25 turning vehicles per hour

- Right turn acceleration lane – greater than 50 turning vehicles per hour and posted speed greater than 40 mph. At signalized intersections where a free right turn is needed to maintain appropriate level of service
- Right turn deceleration and acceleration lanes are generally not required on roadways with three or more travel lanes in the direction of the right turns.
- Left turn acceleration lane – unique factors like highway speed, traffic density, sight distance, nearby traffic control devices, existing auxiliary lanes close to the access, etc. These lanes are generally not required where the posted speed limit is less than 45 mph, the intersection is signalized or the acceleration lane would interfere with the left turn ingress movement.

Based on the criteria listed in the bullets above, one intersection has turning volumes that exceed the thresholds for deceleration lanes where there currently is not a deceleration lane. This is listed below.

- Avenue B-2 & Wallick Road: The westbound left turn exceeds 10 vph. A westbound left turn deceleration lane should be constructed at this intersection.

Sight Distance Evaluation

Entering sight distance (triangle) criteria is listed in Table 5-6-3 within the Laramie County Land Use Regulations and section 6.2 of the WYDOT Traffic Studies Manual. The required sight distance is based on the posted speed of the major roadway, number of travel lanes and the design vehicle. For the purposes of this analysis, it is assumed that each intersection must accommodate a single-unit truck over 10,000 lb gross vehicle weight (GVW).

Wallick Road is a two-lane roadway and South Greeley Highway is a four-lane roadway. At stop-controlled approaches along 25 mph roadways (Wallick), the required sight distance is 325 feet for a single unit truck. Along a WYDOT 50 mph roadways (S. Greeley Highway), the required sight distance is 555 feet for left turning vehicles (looking right) and 480 feet for right turning vehicles (looking left).

Based on a field visit, each of the stop-controlled intersections appears to have adequate entering sight distance.

Traffic Signal Warrant Evaluation

The intersection of South Greeley Highway & Wallick Road was evaluated for traffic signal warrants using the criteria set forth within the Manual on Uniform Traffic Control Devices (MUTCD). The MUTCD provides nine separate warrants (shown below) for evaluating the need for signalization at intersections.

- Warrant 1, Eight-Hour Vehicular Volume
- Warrant 2, Four-Hour Vehicular Volume
- Warrant 3, Peak Hour
- Warrant 4, Pedestrian Volume
- Warrant 5, School Crossing
- Warrant 6, Coordinated Signal System
- Warrant 7, Crash Experience
- Warrant 8, Roadway Network
- Warrant 9, Intersection Near a Grade Crossing

For this intersection, several of the Warrants do not apply. Warrants 1, 2, and 3 apply and were evaluated for these intersections. A 16-hour traffic count was collected the intersection in November 2025. The approach volumes from this count are shown in the table below. Per the MUTCD, the warrant study should consider the effects of the right-turn volume at intersections and if any of the volume should be excluded from the warrant evaluation. The southbound approach provides a right turn lane and the turning vehicles generally do not impact vehicles on Wallick Road from making their maneuver. Therefore, right turn volume for the southbound approach was removed from the warrant calculation.

Table 4 – Signal Warrant: S Greeley Hwy & Wallick 16-hr Volumes

Time	EB	WB	NB	SB	Total Major Rd	Total Intx	Legend
6:00	49	0	419	696	1,115	1,164	Peak Hour
7:00	41	6	528	314	842	889	Peak 4-hour
8:00	121	3	380	245	625	749	Peak 8-hour
9:00	26	5	323	297	620	651	
10:00	22	7	338	251	589	618	
11:00	30	5	393	288	681	716	
12:00	24	8	358	415	773	805	
13:00	37	9	316	390	706	752	
14:00	31	5	344	404	748	784	
15:00	103	8	539	493	1,032	1,143	
16:00	78	7	663	546	1,209	1,294	
17:00	32	9	760	550	1,310	1,351	
18:00	36	2	307	304	611	649	
19:00	20	2	161	204	365	387	
20:00	17	3	125	185	310	330	
21:00	13	1	74	117	191	205	

Warrant 1

Warrant 1 is split into three separate warrants, Warrant 1, 1A and 1B. 1A and 1B are satisfied when, for each of any 8 hours of an average day, the traffic volumes given in the tables below exist on the major street and on the higher-volume minor street approach to the intersection. Warrant 1 is satisfied if 80% of 1A and 1B are satisfied.

**Table 4C-1. Warrant 1, Eight-Hour Vehicular Volume
Condition A—Minimum Vehicular Volume**

Number of lanes for moving traffic on each approach		Vehicles per hour on major street (total of both approaches)				Vehicles per hour on more critical minor-street approach (one direction only)			
Major Street	Minor Street	100% ^a	80% ^b	70% ^c	56% ^d	100% ^a	80% ^b	70% ^c	56% ^d
1	1	500	400	350	280	150	120	105	84
2 or more	1	600	480	420	336	150	120	105	84
2 or more	2 or more	600	480	420	336	200	160	140	112
1	2 or more	500	400	350	280	200	160	140	112

Condition B - Interruption of Continuous Traffic

Number of lanes for moving traffic on each approach		Vehicles per hour on major street (total of both approaches)				Vehicles per hour on more critical minor-street approach (one direction only)			
Major Street	Minor Street	100% ^a	80% ^b	70% ^c	56% ^d	100% ^a	80% ^b	70% ^c	56% ^d
1	1	750	600	525	420	75	60	53	42
2 or more	1	900	720	630	504	75	60	53	42
2 or more	2 or more	900	720	630	504	100	80	70	56
1	2 or more	750	600	525	420	100	80	70	56

^a Basic minimum hourly volume

^b Used for combination of Conditions A and B after adequate trial of other remedial measures

^c May be used when the major-street speed exceeds 40 mph or in an isolated community with a population of less than 10,000

^d May be used for combination of Conditions A and B after adequate trial of other remedial measures when the major-street speed exceeds 40 mph or in an isolated community with a population of less than 10,000

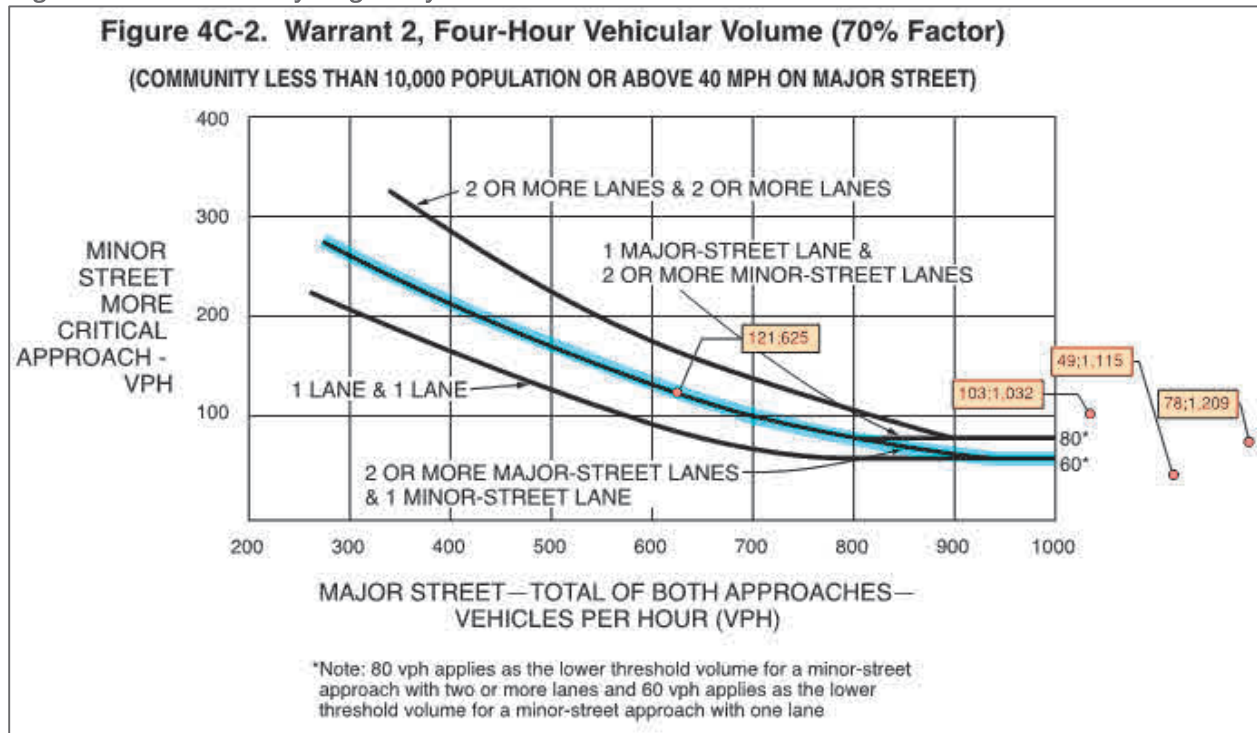
When the 85th percentile speed of the major street exceeds 40 mph in either an urban or rural area, or when the intersection lies within the built-up area of an isolated community having a population less than 10,000, the minimum vehicular volume warrant is 70 percent of the requirements above. This is the case for this intersection, which has a speed limit of 50 mph on South Greeley Highway.

While the major street approach volumes exceed the minimum in the tables shown above, the minor street approaches do not for Warrant 1A. For Warrant 1B, eight of the hours meet the major street volume threshold, but the minor street threshold is not met. Therefore, Warrant 1B is also **not** met.

Warrant 2

The Four-Hour Volume Warrant is satisfied when each of any four hours of an average day the plotted points representing the vehicles per hour on the major street (total of both approaches) and the corresponding vehicles per hour on the higher volume minor street approach (one direction only) all fall above the curve in Figure 4C-2 for the existing combination of approach lanes.

Figure 1 – S. Greeley Highway & Wallick Road – 4-Hour Volume Warrant

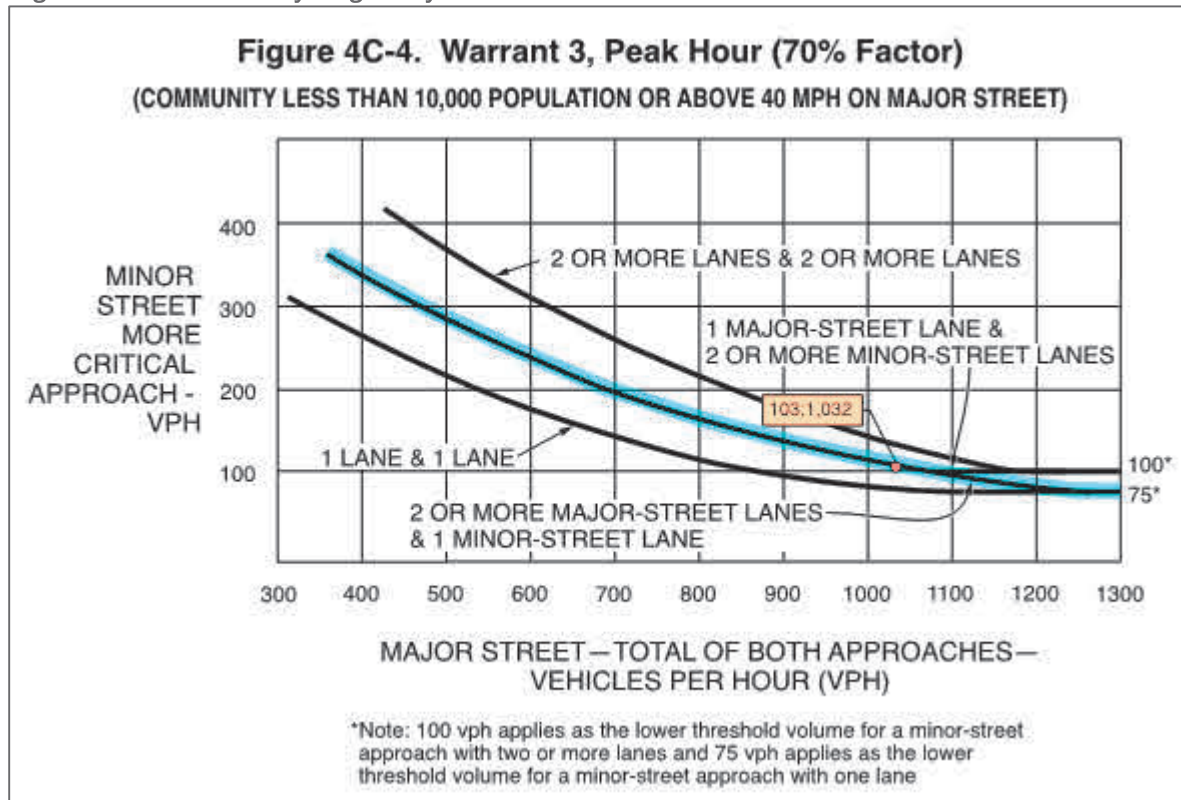


As shown above, only three of the peak four hours fall above the plotted line (highlighted in blue). Therefore, Warrant 2 is **not met** for the existing condition.

Warrant 3

The peak hour volume warrant is satisfied when the plotted point representing the vehicles per hour on the major street (total of both approaches) and the corresponding vehicles per hour of the higher volume minor street approach (one direction only) for one hour (any four consecutive 15-minute periods) of an average day falls above the curve in Figure 4C-4 for the existing combination of approach lanes.

Figure 3 – S. Greeley Highway & Wallick Road – Peak Hour Volume Warrant



As shown above, the peak hour falls just below the blue-highlighted plotted line (2 or more lanes & 1 lane). Therefore, Warrant 3 is **not met** for the existing condition.

Other Warrants

Warrant 4 was not evaluated as a low to moderate amount of pedestrian activity was observed at this intersection. However, there is not a signalized pedestrian crossing of South Greeley Highway for approximately one mile (at College Drive). A signal was also recently installed at High Plains Drive approximately one mile to the south.

Warrant 5 was not evaluated as the intersections is not currently utilized by schoolchildren for crossing US 85 and is not within a school zone.

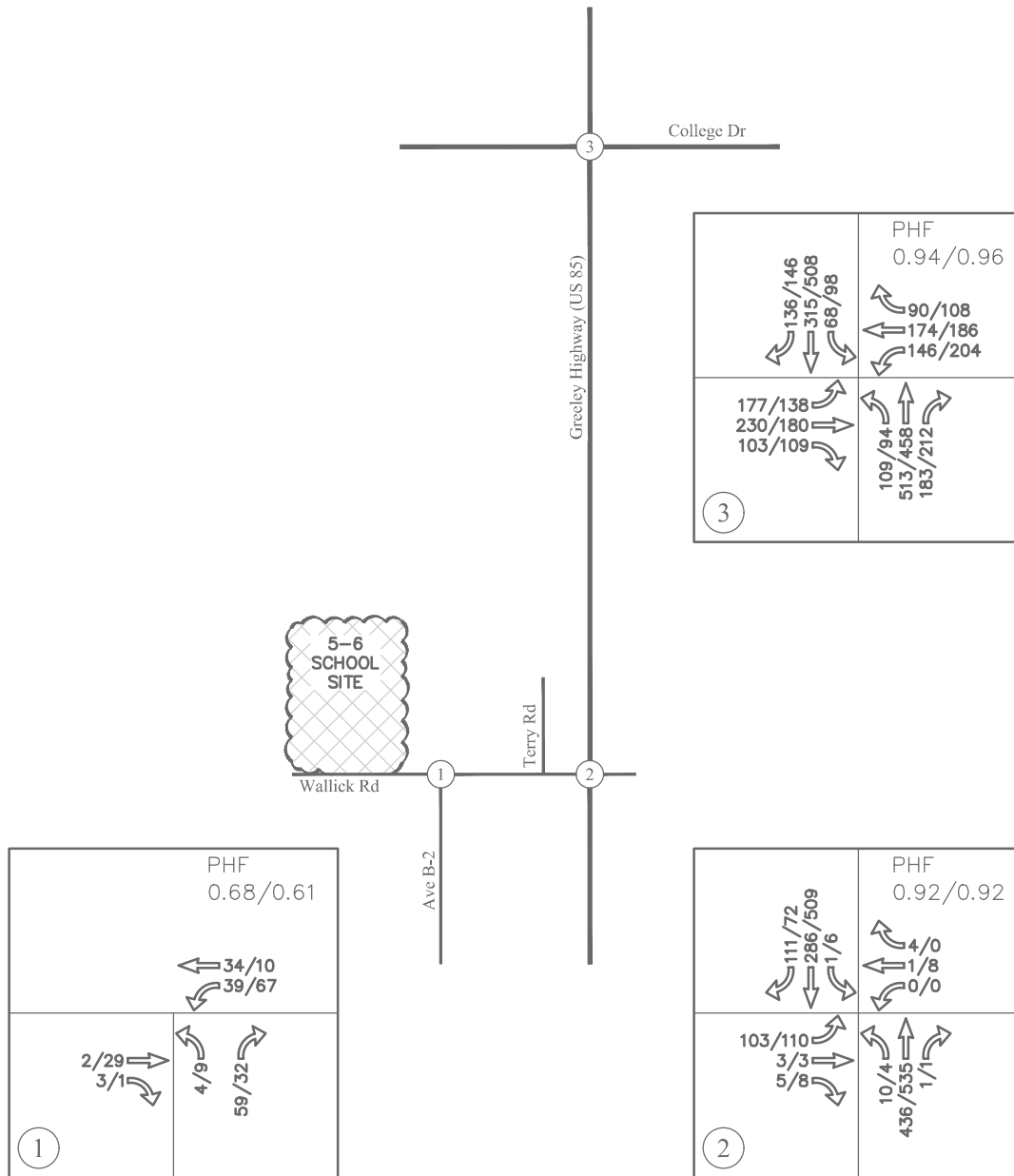
Warrant 6 was not considered as the intersection is not within a coordinated signal system where a signal at this location would be beneficial to maintaining proper platooning of vehicles.

Warrant 7 was not evaluated as the crash data did not indicate significant number of crashes at this location.

Warrant 8 provides justification for installing a traffic signal at some intersection to encourage concentration of traffic flow on a network to create gaps in traffic at upstream unsignalized intersections. With a traffic signal a mile away in each direction, there is adequate number of traffic signals to create gaps in traffic at unsignalized intersections along this corridor.

Warrant 9 relates to railroad proximity. This intersection is not in close proximity to at-grade railroad crossings.

Based on the traffic signal warrant evaluation for the intersection of South Greeley Highway & Wallick Road, no warrants are met for signaling this intersection currently. Meeting a single or multiple traffic signal warrant does not inherently mean a traffic signal shall be installed but can provide justification for implementation. Installation of a traffic signal does not inherently make an intersection safer. In fact, traffic signals often increase the number of crashes at inter



12/34 – AM Peak Hour/Dismissal Hour
 PHF – Peak Hour Factor

Source: Traffic counts collected by All Traffic Data Services on November 5, 2025, a Wednesday.



NOT TO SCALE
 FEBRUARY 4, 2026

EXHIBIT 5

EXISTING (2025) TRAFFIC VOLUMES



TRIP GENERATION AND DISTRIBUTION

Trip Generation

The project is proposing to build a new school that will accommodate 5th and 6th graders in the south triad of the Laramie County School District near Cheyenne, Wyoming. The school is anticipated to have a capacity of 650 students and will operate with an initial enrollment of 632 students. Laramie County School District typically operates their schools up to 90% capacity, which would equate to 585 students at this school.

Approximately 90 of the 5th and 6th grade students currently attend Afflerbach Elementary, immediately east of this site, and are already creating trips to the area. Therefore, the net increase in students to this area is estimated to be 560 students (at capacity).

The *Trip Generation Manual, 12th Edition* published by the Institute of Transportation Engineers (ITE) was used to estimate the number of trips generated by the proposed land use. The purpose of the Trip Generation Manual (TGM) is to compile and quantify empirical trip generation rates for specific land uses within the US, UK and Canada. The proposed school facility falls under the land use category (LUC) 522 "Middle / Junior High School." The trip generation for proposed project is shown in the table below.

Table 5 - Trip Generation Estimate per ITE *Trip Generation*

Land Use	ITE Code	Size (stud.)	Daily Trips	AM Peak Hour			PM Peak Hour		
				In	Out	Total	In	Out	Total
Middle School	522	650	1,412	283	232	515	104	121	225
Middle School	522	560	1,222	246	202	448	90	105	195

As shown in the table above, the proposed 5/6 school is estimated to generate up to 1,412 daily vehicle trips with 515 occurring during the arrival (AM) peak hour and 225 during the dismissal (PM) hour trips. However, approximately 90 of the students that will attend this school currently attend Afflerbach Elementary and already travel to this area. Therefore, the net increase in trips to Wallick Road is estimated to be approximately 1,222 daily trips with 448 during the AM peak hour and 195 during the dismissal hour.

Trip Types

Nearly all developments are made up of the following six trip types: new (destination) trips, pass-by trips, diverted trips, shared (internal) trips, multi-modal (non-vehicular) trips, and transit-oriented trips. In order to better understand the trip types available for land access and how they relate to this project, a description of each specific type follows.

New (Destination) Trips – These types of trips occur to access a specific land use such as a new retail development or a new residential subdivision. These types of trips will travel to and from the new site and a single other destination such as home or work. This is the only trip type that will result in a net increase in the total amount of traffic within the

study area. The reason primarily is that these trips represent planned trips to a specific destination that never took trips to that part of the city prior to the development being constructed and occupied. This project will develop new trips.

Pass-by Trips – These trips represent vehicles which currently use adjacent roadways providing primary access to new land uses or projects. These trips, however, have an ultimate destination other than the project in question. They should be viewed as drop-in customers who stop in on their way home from work. A good example is a quick stop at the grocery store to pick up an ingredient for dinner on the way home from work or at a latte stand to grab a coffee on the way to work. This can make this trip pre-determined, but the stop is still on the way by. Another example would be on payday, where an individual generally drives by their bank every day without stopping, except on payday. On that day, this driver would drive into the bank, perform the prerequisite banking and then continue home. In this example, the trip started from work with a destination of home, however on the way, the driver stopped at the grocery store/latte stand and/or bank directly adjacent to their path. Pass-by trips are most always associated with commercial/retail types of developments. Therefore, pass-by trips are not anticipated for this project, but could occur if a parent drops their student off or picks them up during their home-work / work-home commute.

Diverted (Linked) Trips - Diverted trips are like pass-by trips, but diverted trips occur from roadways that do not provide direct access to the site. Instead, one or more streets must be utilized to get to and from the site. Similar to pass-by trips, diverted trips are most always associated with commercial/retail type developments. Diverted trips for this project could occur from South Greeley Highway, College Drive or any other roadway that does not provide direct access to the site. Given the type of development and the difficulty to verify these types of trips, diverted trips were not accounted for in the analysis.

Shared Trips - Internal trips are the portion of trips generated by a mixed-use development that both begin and end within the development. When estimating trip generation for a development with several uses, each use will generate its own trips. If those trips occur between two of the onsite uses without using the external roadway system, it is considered a shared or internal trip. This trip type reduces the number of new trips generated on the public road system and is most commonly used for commercial or mix-use developments. Determining these trip types is more difficult to quantify and without specific guidance are usually determined by engineering judgment on a project-by-project basis. For this project, the school is the only use on the site. Therefore, shared (internal) trip are not anticipated, though they could occur between this school and the adjacent elementary.

Multi-Modal Trips - These are non-vehicular trips to and from the site, mostly comprised of pedestrian and bicycle trips. Generally, they are local trips from the surrounding neighborhood or adjacent businesses. If a development is in an area with a high amount of bicycle and pedestrian activity, such as a downtown setting or college campus, a reduction of vehicular trips would be anticipated. Pedestrian and bicycle trips are anticipated for this project and would be most common for students that live within ½ mile

of the school, which would primarily be north and south of the school in the manufactured home communities.

Transit Trip – Laramie County is served with public bus by Cheyenne Transit Program (CTP). The nearest bus stop to the site is at S. Greeley Highway & Allison Road, nearly two (2) miles from the school site. Therefore, transit trips are unlikely to occur for school staff. Laramie County School District will also operate school bus transportation for students that live more than ½ mile from the school. This school will operate several buses and parking on site is provided with access from Wallick Road.

Based on the various trip types depicted above and the nature of the proposed project, no reduction applied to the trip generation estimates shown in Table 5.

Trip Distribution

As shown on the site plan, the site is proposed to provide one ingress-only access and one egress-only access to Wallick Road for student drop-off / pick-up and parking (staff and visitors). A separate full-movement access to Wallick Road is proposed for school buses.

The new school will shift all of the 5th and 6th grade students from existing elementary schools within the South Triad area of the district. The elementary schools will serve kindergarten (or pre-K) through 4th grade once the new school is completed and operational. The south triad includes all areas east of I-25 and south of I-80 within the district as well as some portions north of I-80 that currently attend Cole, Hebard, Fairview, Lehart, Bain and Sunrise Elementary Schools. The anticipated school service boundary along with a student address heat map was utilized to estimate general trip distribution for the elementary. The vast majority of students will live north of the site and will utilize South Greeley Highway to travel to/from the school. A portion of the students (<5%) live within ½ mile of the school and may walk. With added road connections and residential development in the long-range conditions, a slightly different distribution is anticipated.

The following estimated trip distribution was utilized for the proposed school:

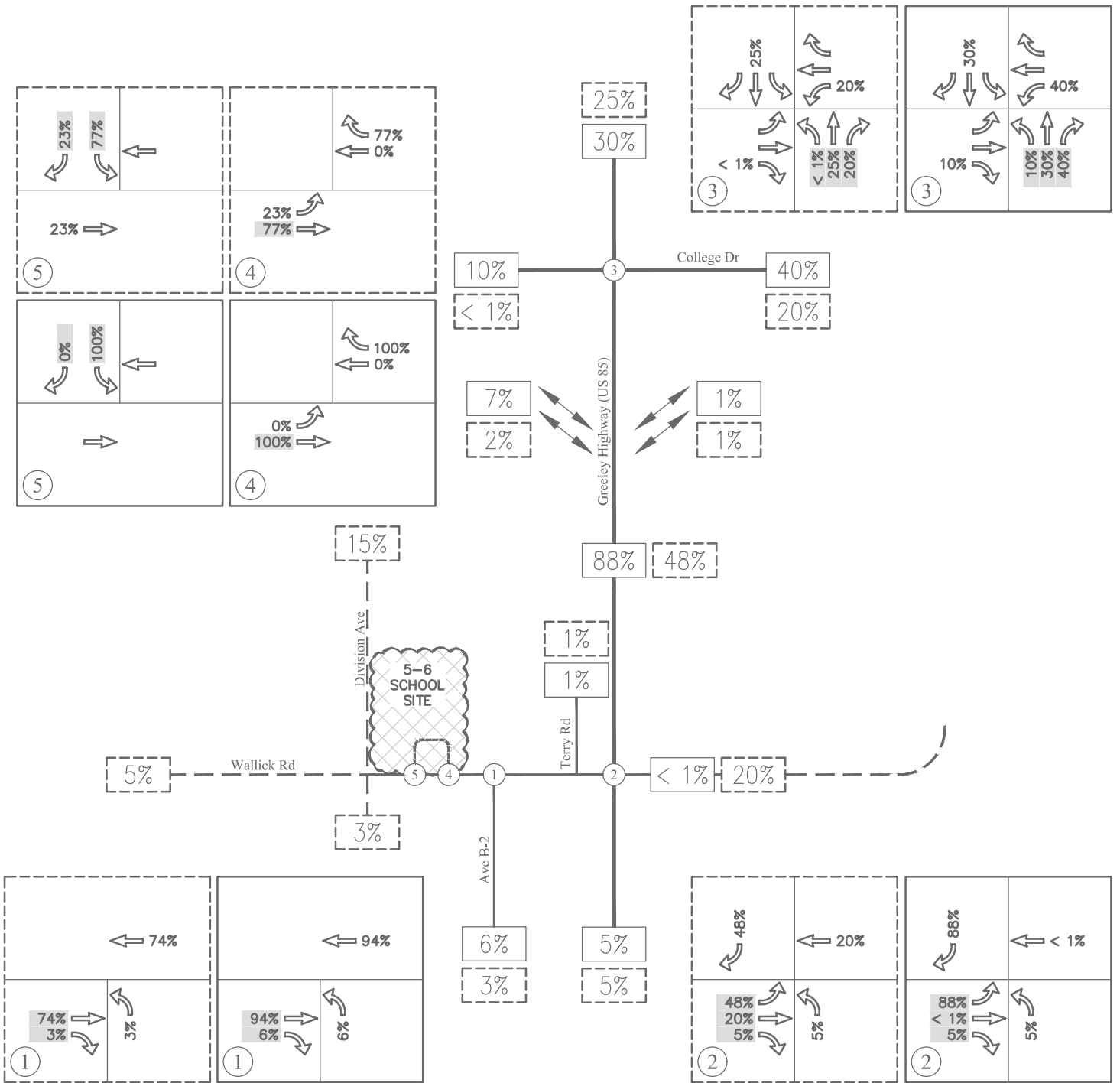
Short-term

- South Greeley Highway north of Wallick Road – 88%
 - Neighborhoods west of US 85 between Wallick Rd and College Dr – 7%
 - Neighborhoods east of US 85 between Wallick Rd and College Dr – 1%
 - College Drive west of US 85 – 10%
 - College Drive east of US 85 – 40%
 - South Greeley Highway north of College Dr – 30%
- South Greeley Highway south of Wallick Road – 5%
- Terry Road north of Wallick Road – 1%
- Avenue B-2 south of the site – 6%

Long-range

- South Greeley Highway north of Wallick Road – 48%
 - Neighborhoods west of US 85 between Wallick Rd and College Dr – 2%
 - Neighborhoods east of US 85 between Wallick Rd and College Dr – 1%
 - College Drive west of US 85 – < 1%
 - College Drive east of US 85 – 20%
 - South Greeley Highway north of College Dr – 25%
- South Greeley Highway south of Wallick Road – 5%
- Terry Road north of Wallick Road – 1%
- Avenue B-2 south of the site – 3%
- Wallick Road west of Division – 5%
- Division Ave north of Wallick – 15%
- Division Ave south of Wallick – 3%
- Wallick Road east of US 85 – 20%

The trip distribution percentages are illustrated in **Exhibit 6**. The project generated trips at the study area intersections are shown in **Exhibits 7 and 8**.



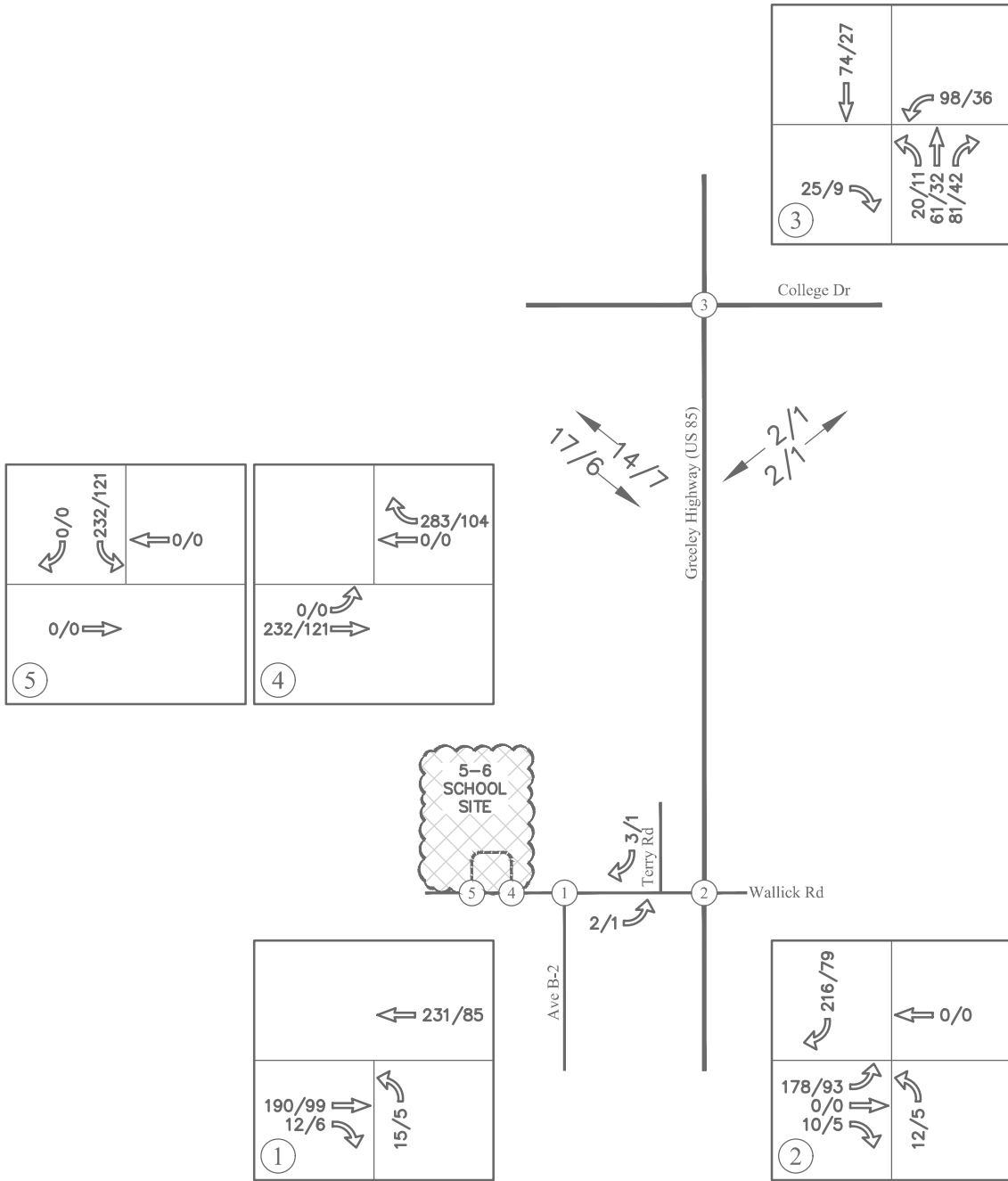
- XX% - GENERAL TRIP DISTRIBUTION (SHORT-TERM)
- XX% - GENERAL TRIP DISTRIBUTION (LONG-RANGE)
- PROJECT GENERATED INGRESS TRIPS (%)
- PROJECT GENERATED EGRESS TRIPS (%)
- - LONG-RANGE ROADWAYS



NOT TO SCALE
FEBRUARY 4, 2026

EXHIBIT 6
PROJECT TRIP DISTRIBUTION





12/34 – AM Peak Hour/Dismissal Hour

Note: Volumes between the site access and other intersection on Wallick Road may not balance. The site driveway volumes are based on the 650 student capacity trip generation and other study area intersections are based on the 560 students net increase to the area (90 already at Afflerbach).

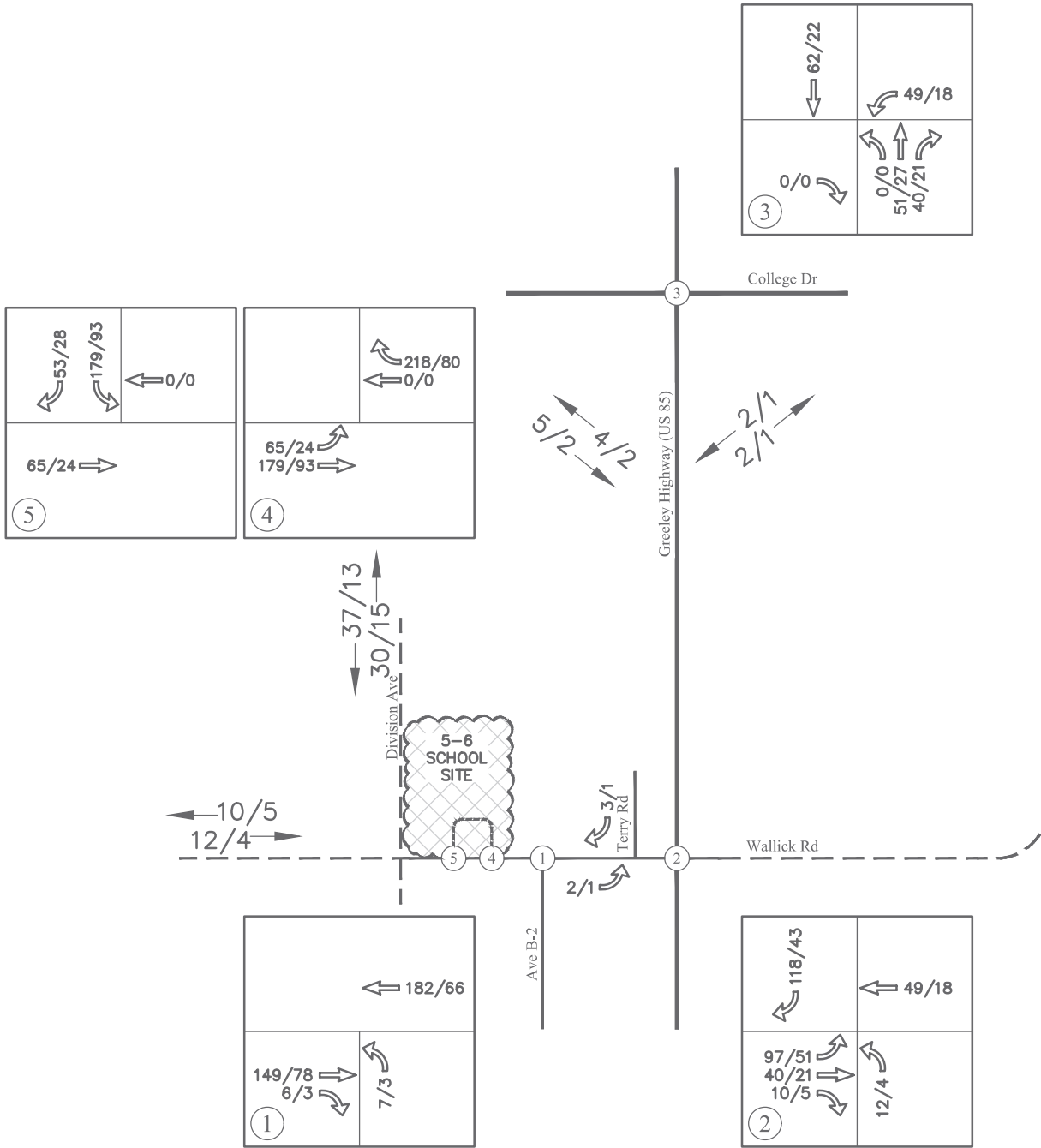


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FEBRUARY 4, 2026

EXHIBIT 7

SHORT-TERM SITE-GENERATED TRIPS





12/34 – AM Peak Hour/Dismissal Hour

Note: Volumes between the site access and other intersection on Wallick Road may not balance. The site driveway volumes are based on the 650 student capacity trip generation and other study area intersections are based on the 560 students net increase to the area (90 already at Afflerbach).

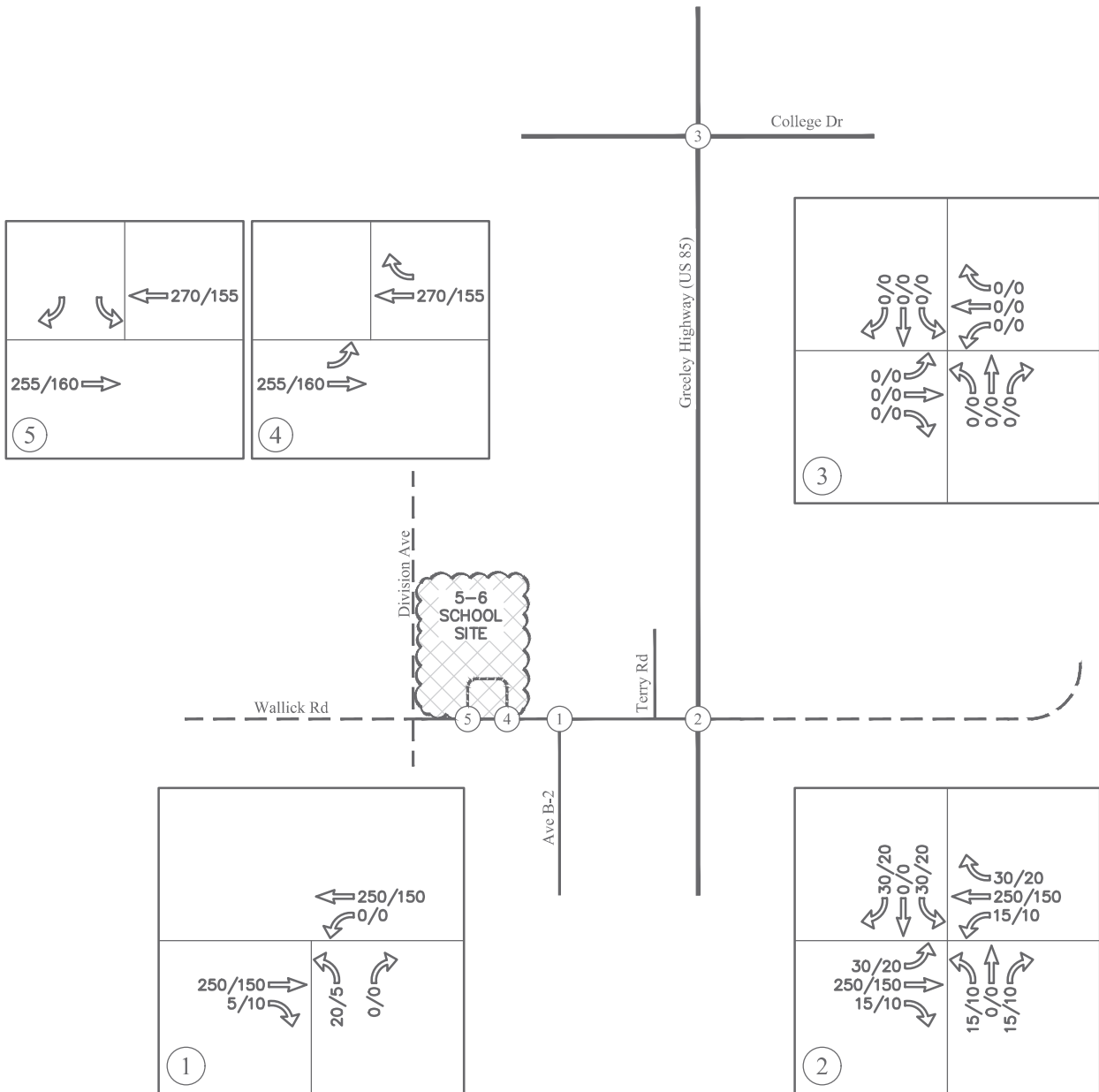


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FEBRUARY 4, 2026

EXHIBIT 8

LONG-RANGE SITE-GENERATED TRIPS





12/34 – AM Peak Hour/Dismissal Hour

NOTE: Trips are a rough estimate based on Cheyenne MPO ADT projections and potential development along Division Avenue, Wallick Road (west of site and east of US 85), and Avenue C.



NOT TO SCALE
FEBRUARY 4, 2026

EXHIBIT 9

**FUTURE ROADWAY CONNECTIONS (DIVISION, WALLICK)
TRIP ADJUSTMENT**



FUTURE YEAR TRAFFIC IMPACT ANALYSIS

Level of service calculations for the short-term (Year 2029) and long-range (Year 2050) conditions assumed that the existing traffic volumes as shown on Exhibit 5 experience a background increase above the 2025 volumes as described in the “Ambient Growth” section of this report (2% College Drive, 0.5% all other roadways). Two scenarios were examined for each of the future condition, one without the project and one with the project completed. A list of the future scenarios follows.

- Short-term Condition (Year 2029) without Project
- Short-term Condition (Year 2029) with Project
- Long-range Condition (Year 2050) without Project
- Long-range Condition (Year 2050) with Project

These scenarios will allow a specific comparison of impacts to the study area intersections and allow a determination to be made as to the extent of the project’s impact and if any mitigation measures will be required.

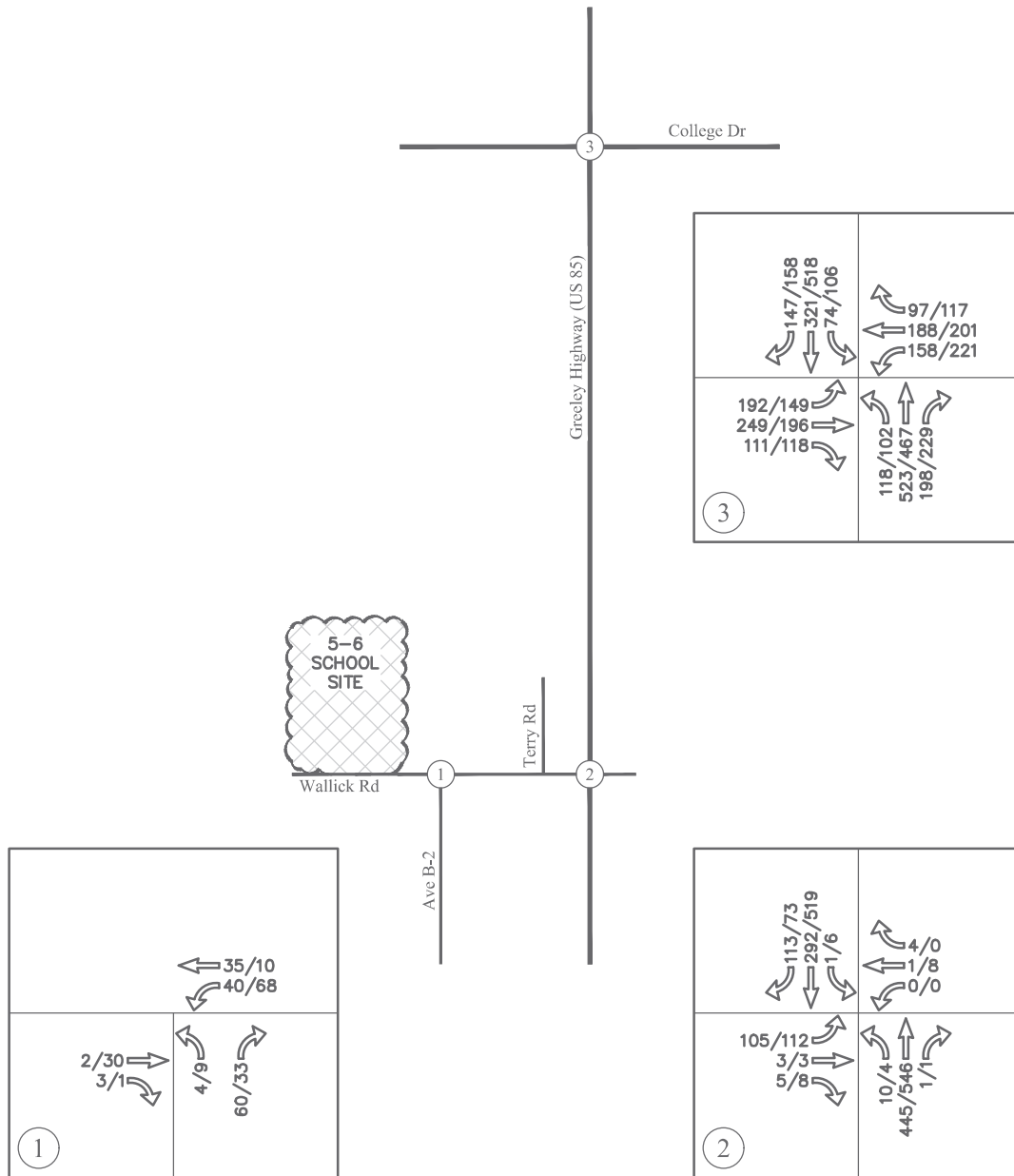
Short-Term Condition (Year 2029) without Project

The traffic volumes for this scenario include the existing (2025) traffic volumes as shown on Exhibit 5 and four years of ambient growth. The total traffic volumes anticipated under this condition are shown on Exhibit 10. A summary of the results is shown in Table 6 below

Table 6 - Year 2029 Levels of Service without Project

INTERSECTION (S)ignalized (U)nsignalized		Approach Or Overall	AM Peak		Dismissal Peak	
			Delay (sec)	LOS	Delay (sec)	LOS
Avenue B-2 & Wallick Rd	U	NB	8.8	A	9.3	A
S. Greeley Hwy & Wallick Rd	U	EB	18.2	C	39.3	E
		WB	11.7	B	27.7	D
S. Greeley Hwy & College Dr	S	Ovr	24.5	C	21.9	C

With the anticipated increase in traffic over the next four years, the intersections within the study area are anticipated to continue to operate at similar levels as existing conditions. The eastbound approach of S. Greeley Highway & Wallick Road is anticipated to continue to operate at LOS E during the dismissal hour.



12/34 – AM Peak Hour/Dismissal Hour



NOT TO SCALE
FEBRUARY 4, 2026

EXHIBIT 10

SHORT-TERM (2029) NO PROJECT TRAFFIC VOLUMES



Short-Term Condition (Year 2029) with Project

The traffic volumes for this scenario include the existing (2025) traffic volumes as shown on Exhibit 5, four years of ambient growth, and the additional traffic from the proposed project, as shown on Exhibit 7. Wallick Road construction, which will widen the roadway to a 3-lane section from S. Greeley Highway to Division Avenue is anticipated to be completed for this scenario. The total traffic volumes anticipated under this condition are shown on Exhibit 11. A summary of the results is shown in Table 7 below.

Table 7 - Year 2029 Levels of Service with Project

INTERSECTION (S)ignalized (U)nsignalized		Approach Or Overall	AM Peak		Dismissal Peak	
			Delay (sec)	LOS	Delay (sec)	LOS
Avenue B-2 & Wallick Rd	U	NB	12.2	B	10.5	B
S. Greeley Hwy & Wallick Rd	U	EB	54.3	F	124.7	F
		WB	13.0	B	31.3	D
S. Greeley Hwy & College Dr	S	Ovr	28.6	C	23.3	C

With the addition of traffic generated by the proposed project, all of the intersections within the study area are anticipated to continue to operate within acceptable levels except the eastbound approach of S. Greeley Highway & Wallick Road, which is anticipated to fall to LOS F during the AM peak and dismissal hours.

Speed Change Lane Evaluation

With the addition of the project traffic, the site ingress access is anticipated to meet requirements for a right turn deceleration lane. There will be little need for it initially as Wallick Road will dead-end at Division Avenue. However, once Wallick Road is extended west and Division Avenue is constructed, the right turn deceleration lane and a left turn deceleration lane will be required.

Sight Distance

The plans for the Cheyenne 5/6 School show two new driveways to Wallick Road that will accommodate egress traffic, one for the parking lot and drop-off / pick-up operation and one for the buses. Wallick Road is assumed will operate at 30 mph once completed as a major collector. The required sight distance along a 30 mph 2-lane roadway is 300' for passenger cars and pickup trucks and 390 feet for single unit trucks over 10,000 lbs. It is assumed a bus requires similar sight distance as a single unit truck. These sight lines should be shown on the construction plans. The resulting sight triangle shall be kept free from obstructions to vision between the heights of 2.5' and 12' above the street grades.

Traffic Signal Warrant Evaluation

As shown in the existing conditions section, the intersection of South Greeley Highway & Wallick Road was evaluated for traffic signal warrants (Warrants 1, 2 and 3) using the criteria set forth within the MUTCD.

The estimated 16-hour traffic volumes for the intersection with the project traffic added are shown in the table below. As with the existing conditions, the turn volume for the southbound approach was removed from the warrant calculation.

Table 8 – Signal Warrant: S Greeley Hwy & Wallick 16-hr Volumes (Short-term Build)

Time	EB	WB	NB	SB	Total Major Rd	Total Intx
6:00	55	0	428	710	1,138	1,193
7:00	188	6	549	320	869	1,063
8:00	201	3	392	250	642	846
9:00	36	5	330	303	633	674
10:00	30	7	345	256	601	638
11:00	39	5	401	294	695	739
12:00	33	8	365	423	788	829
13:00	49	9	322	398	720	778
14:00	56	5	354	412	766	827
15:00	212	8	553	503	1,056	1,276
16:00	116	7	678	557	1,235	1,358
17:00	78	9	777	561	1,338	1,425
18:00	80	2	316	310	626	708
19:00	60	2	165	208	373	435
20:00	50	3	128	189	317	370
21:00	19	1	75	119	194	214

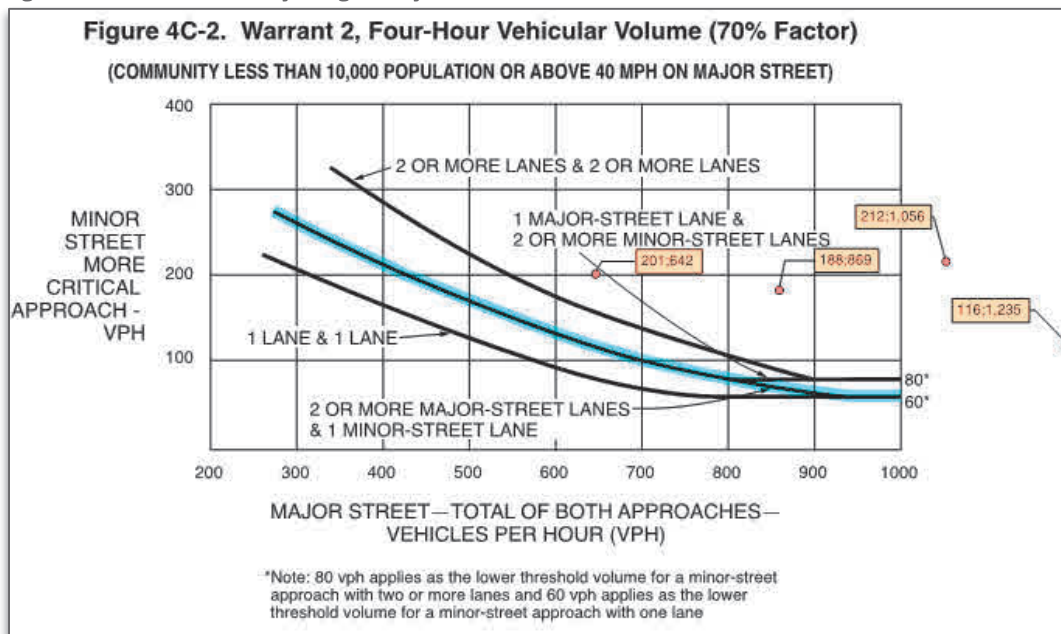
Legend
Peak Hour
Peak 4-hour
Peak 8-hour

Warrant 1A: There are only four hours that are anticipated to exceed the 105 vph minor-street threshold in this warrant. Therefore, Warrant 1A is not anticipated to be met.

Warrant 1B: There are eight hours that are anticipated to exceed the 53 vph threshold on the minor street, but there is insufficient projected traffic on the major roadway during those hours to meet the warrant. Therefore, Warrant 1B is not anticipated to be met.

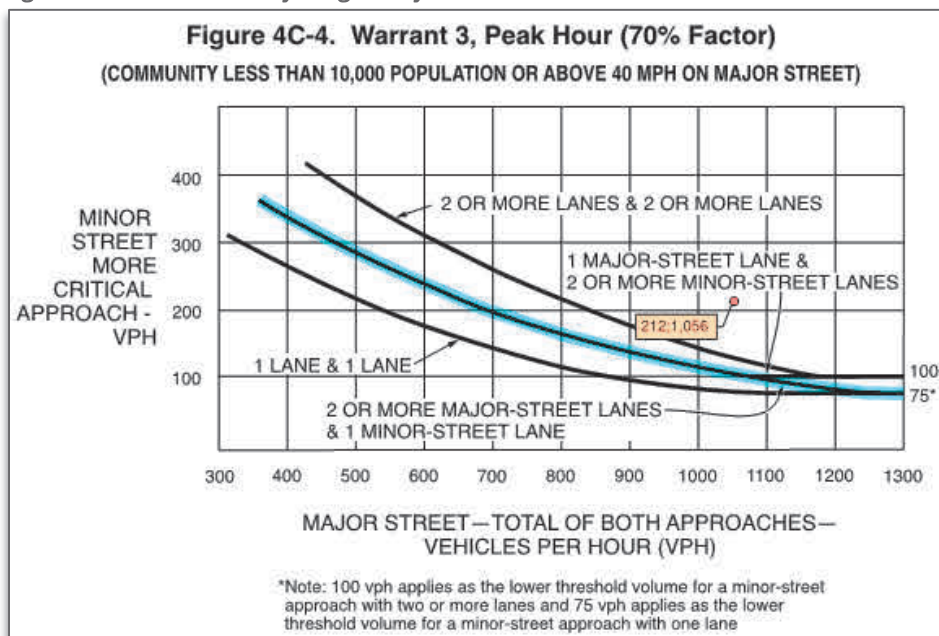
Warrant 2: There are four hours that are estimated to fall above the line plotted in Figure 4C-2 in the MUTCD (shown below). Therefore, Warrant 2 is anticipated to be **met**.

Figure 4 – S. Greeley Highway & Wallick Road – 4-Hour Volume Warrant



Warrant 3: The peak hour, which was nearly met in the existing conditions is anticipated to be **met** for the short-term with project scenario as shown in the following figure.

Figure 5 – S. Greeley Highway & Wallick Road – Peak Hour Volume Warrant



With two of the MUTCD volume warrants for signalization anticipated to be met once the school is constructed and operational, a traffic signal should be considered for installation at this intersection. Installation of a traffic signal is anticipated to alleviate delay and level of service deficiencies at the intersection.

Safe Routes to School – Pedestrian and Bicycle Infrastructure

Stantec completed a Safe Routes to School (SRTS) plan update for the Cheyenne Metropolitan Planning Organization on July 17, 2025. The plan presents recommendations for each school in the metro area including Afflerbach Elementary. It identifies existing sidewalk and greenway trail infrastructure as well as sidewalk network gaps. Since the proposed 5/6 school is adjacent to Afflerbach Elementary, the recommendations for Afflerbach are taken into consideration for the proposed school. The plan identifies three safety issues and provides recommendations for potential improvements. These are listed below.

Intersection of W Wallick Rd and S Greeley Hwy:

Safety Issue: Intersection lacks safe crossing opportunity for non-motorists as the crossing width is wide and traffic speed on S Greeley Hwy is fast. During the study's public engagement events, many residents living on the east side of Greeley Hwy have expressed difficulty and safety concerns crossing the corridor.

Consider: Mark crossing with high visibility paint markings; Install bollards an/or in-street signing; Install crossing signal; Construct curb extensions (Priority High)

S Greeley Hwy / US 85

Safety Issue: Corridor experiences high traffic speeds and lacks pedestrian connections, contributing to unsafe crossing opportunities and access to areas on either side of the corridor; S Greeley Hwy is the only north-south route in the area.

Consider: Narrow width of drive lanes to minimum standard; Reduce the number of parallel lanes; Install vertical streetscape elements (Priority High)

All identified gaps within the existing pedestrian and bicycle network (i.e. sidewalk, trail, etc.)

Safety Issue: Gaps within the sidewalk/bicycle/trail network discourage active transportation and often pose safety concerns for non-motorists.

Consider: Wherever opportunities rise (programmed capital improvement projects, new developments, etc.) the responsible roadway jurisdiction(s) should always consider eliminating existing gaps within the sidewalk and bicycle networks to improve safety for active transportation users. (Priority Low/Medium)

Below is the infrastructure map shown in the SRTS plan update.

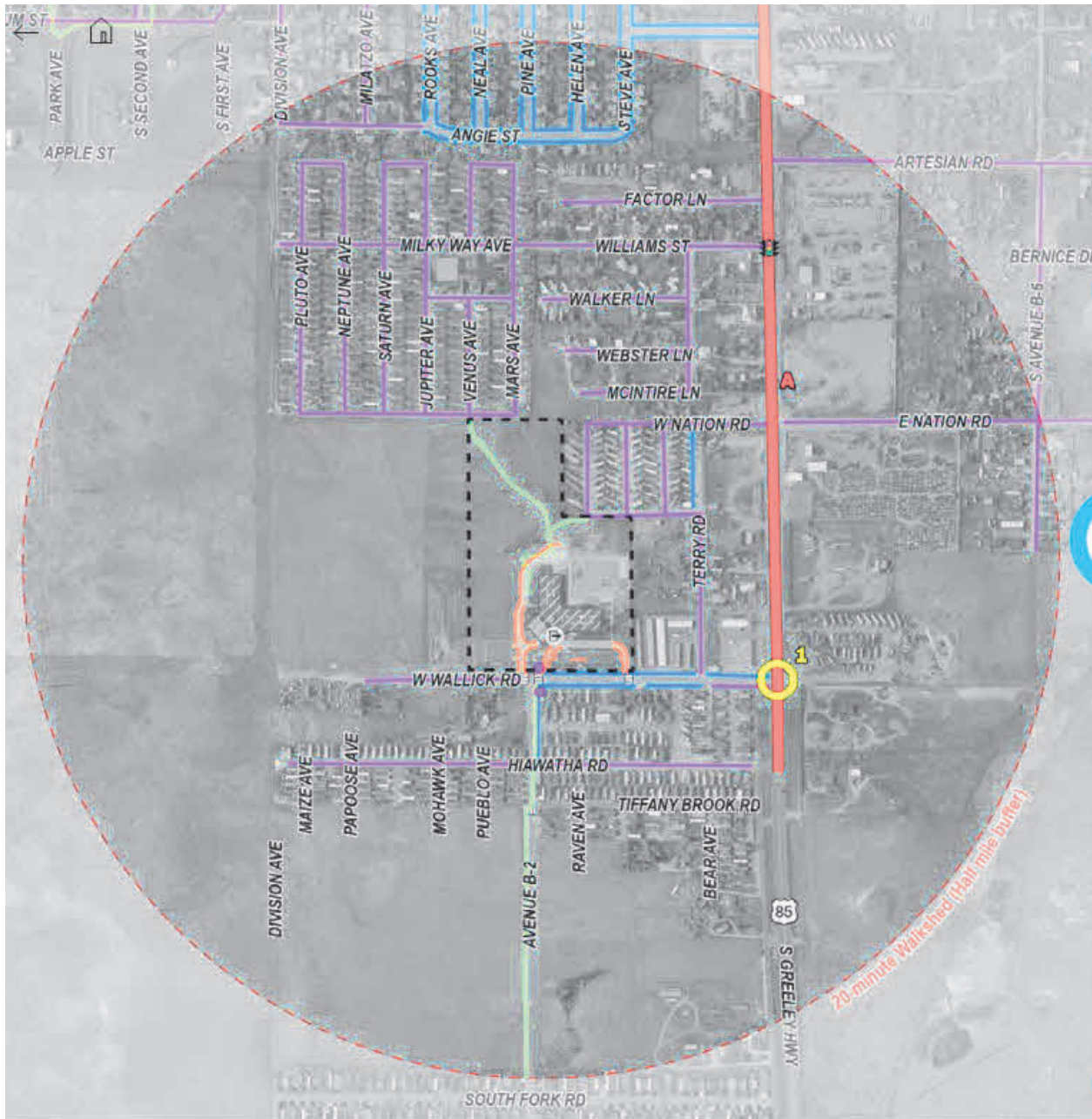


Figure 68: Afflerbach Recommendations Map

1000 feet

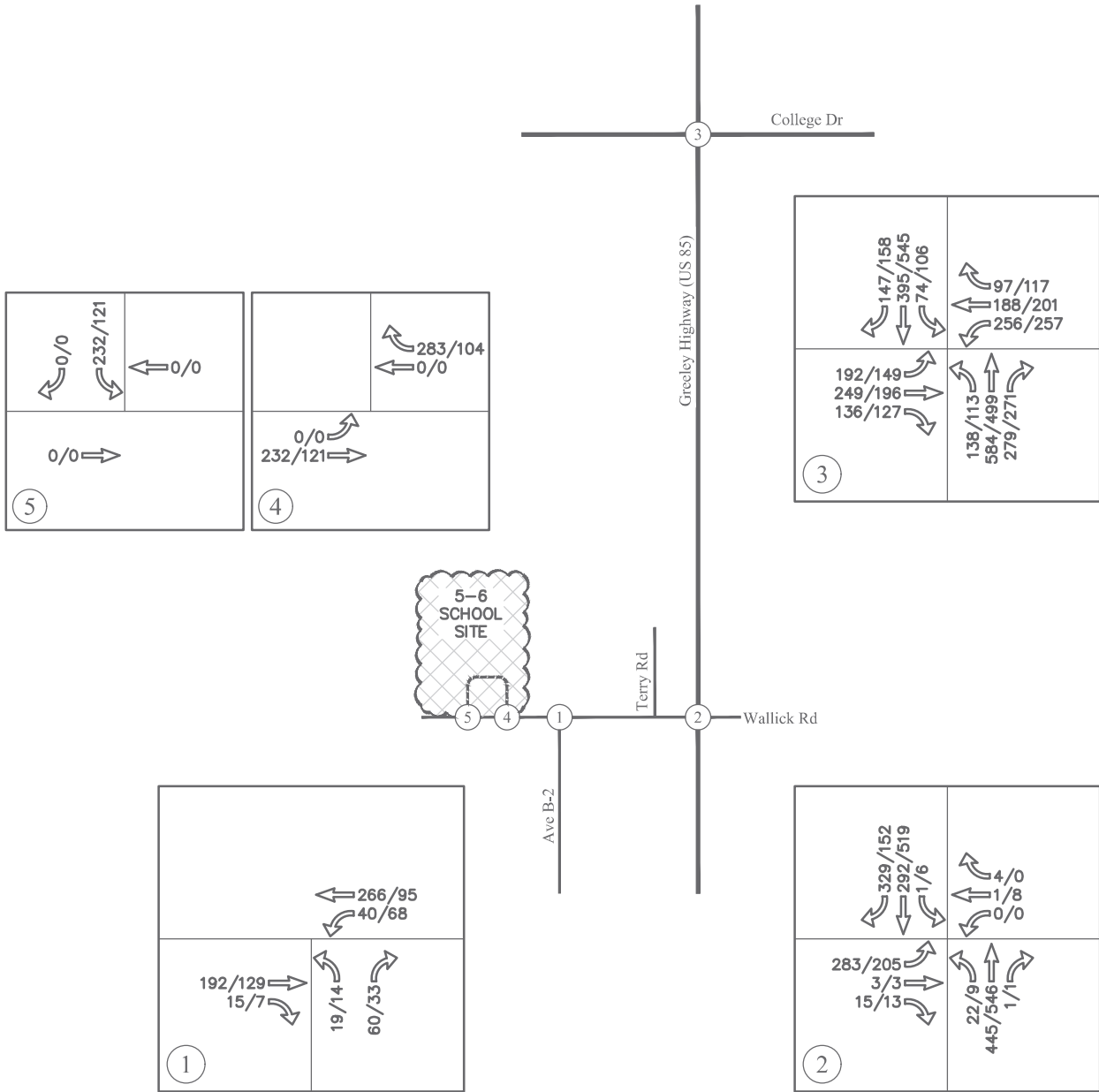
North

3.1 Afflerbach Elementary

- | | | | |
|-------------------------------|---------------------------------|---------------------|--|
| School Yard | No Parking (Loading Only) | Stop Sign | Flashing Beacon (RRFB) |
| School Building Footprint | Existing Greenway & Trail | Yield Sign | Hybrid Beacon (PHB) |
| Main Entry/Exit Door | Planned Greenway | School Zone Flasher | Safety Improvement Opportunity (Spot location) |
| 20-Minute Walkshed (0.5-mile) | Existing Sidewalk | Traffic Signal | Safety Improvement Opportunity (Segment) |
| | Identified Sidewalk Network Gap | Marked Crosswalk | |

School Speed Zones

A 20-mph school speed zone is already in place along Wallick Road, which is posted for westbound traffic at Terry Road. There is not a posted school speed zone for eastbound Wallick Road, but rather a speed limit sign on northbound Avenue B-2 approximately 150' south of Wallick. A separate 20-mph school zone sign should be installed for eastbound Wallick Road near the proposed 5/6 school. Once Division Avenue is constructed, school speed zone signing should be considered for this roadway as well.



12/34 – AM Peak Hour/Dismissal Hour



NOT TO SCALE
FEBRUARY 4, 2026

EXHIBIT 11

SHORT-TERM (2029) WITH PROJECT TRAFFIC VOLUMES



Long-range Condition (Year 2050) without the Project

This section focuses on the long-range scenario of the year 2050 without traffic from the project. This scenario assumes that the proposed project has not developed, but the ambient growth continues at 2% along College Drive and 0.5% elsewhere for the next 25 years. Additionally, the Wallick Road, Division Avenue and Avenue C connections in the vicinity of the study area are anticipated to be completed along with development along these corridors. Additionally, once Wallick Road is extended to the east of S Greeley Highway to Avenue C and beyond, the intersection of S Greeley Highway & Wallick Road will require a traffic signal regardless of whether the proposed 5/6 school project moves forward or not. Therefore, it is assumed to be installed for this scenario.

This analysis will show how the traffic volumes will be handled by the transportation facilities and what improvements may be required to accommodate the anticipated growth in traffic within the study area. The traffic volumes for this condition include the existing traffic, as shown on Exhibit 5 with the ambient background growth as outline within the “Ambient Traffic Growth” section of this report plus the estimated trips associated with the Wallick Road extension to the east and west as shown on Exhibit 9. Please see Exhibit 12 for the traffic volumes used for this scenario. A summary of the level of service results is shown in Table 9, which follows.

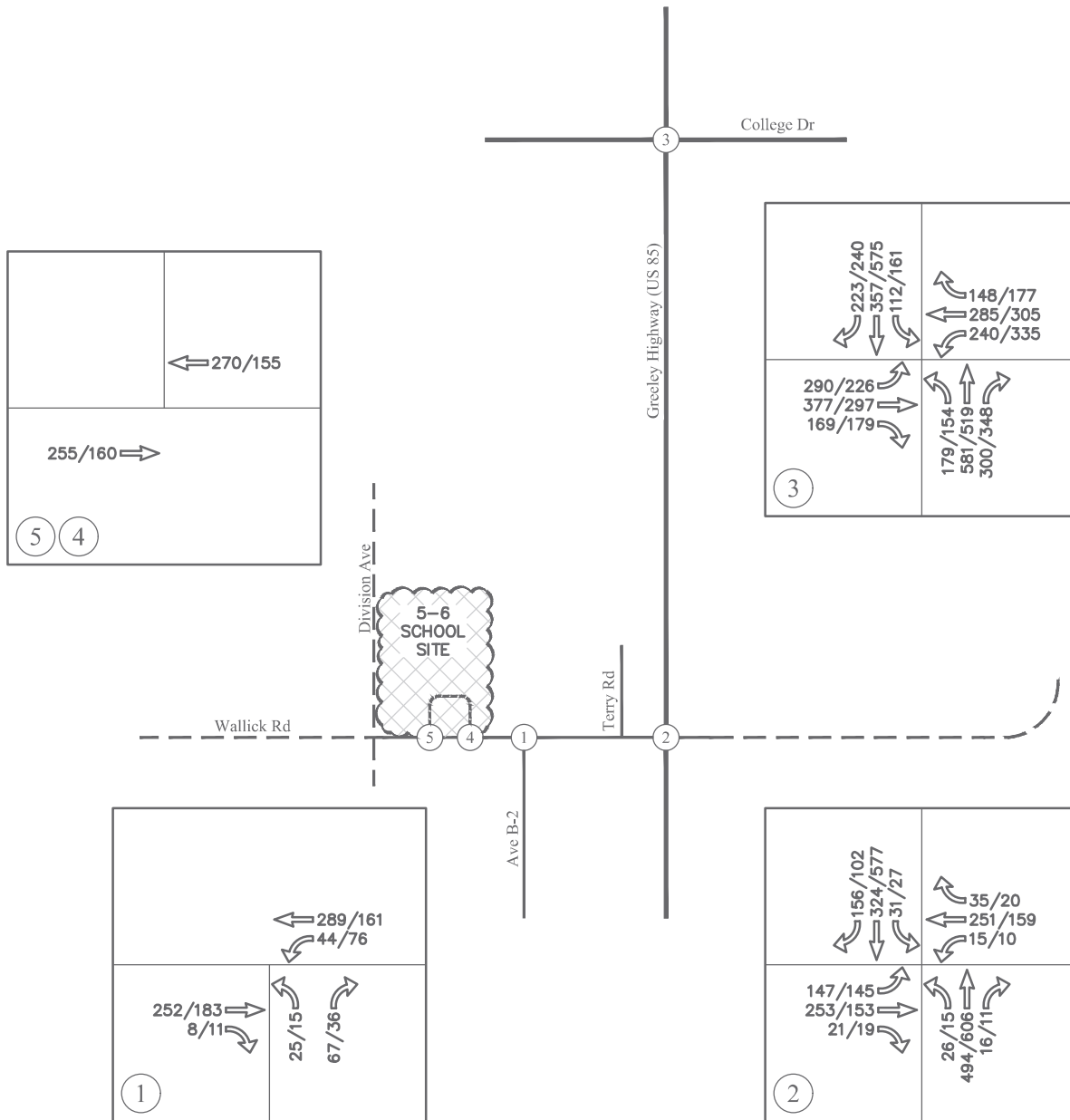
Table 9 - Year 2050 Levels of Service Without the Project

INTERSECTION (S)ignalized (U)nsignalized		Approach Or Overall	AM Peak		Dismissal Peak	
			Delay (sec)	LOS	Delay (sec)	LOS
Avenue B-2 & Wallick Rd	U	NB	12.2	B	10.8	B
S. Greeley Hwy & Wallick Rd	S	Ovr	21.2	C	17.0	B
S. Greeley Hwy & College Dr	S	Ovr	48.5*	D*	32.8	C

*one or more movement is shown to operate at LOS F

With the anticipated growth in traffic within the study area over the next 25 years and the added traffic from the extension of Wallick Road and associated development, the study area intersections are anticipated to operate at acceptable levels of service. The intersection of S Greeley Highway & College Drive is shown to have approaches operating at LOS F for this scenario, which are anticipated to be able to be addressed with signal timing modifications.

The level of service reports for the long-range no project conditions are provided in the technical appendix.



12/34 – AM Peak Hour/Dismissal Hour



NOT TO SCALE
FEBRUARY 4, 2026

EXHIBIT 12

LONG-RANGE (2050) NO PROJECT TRAFFIC VOLUMES



Long-range Conditions (Year 2050) with the Project

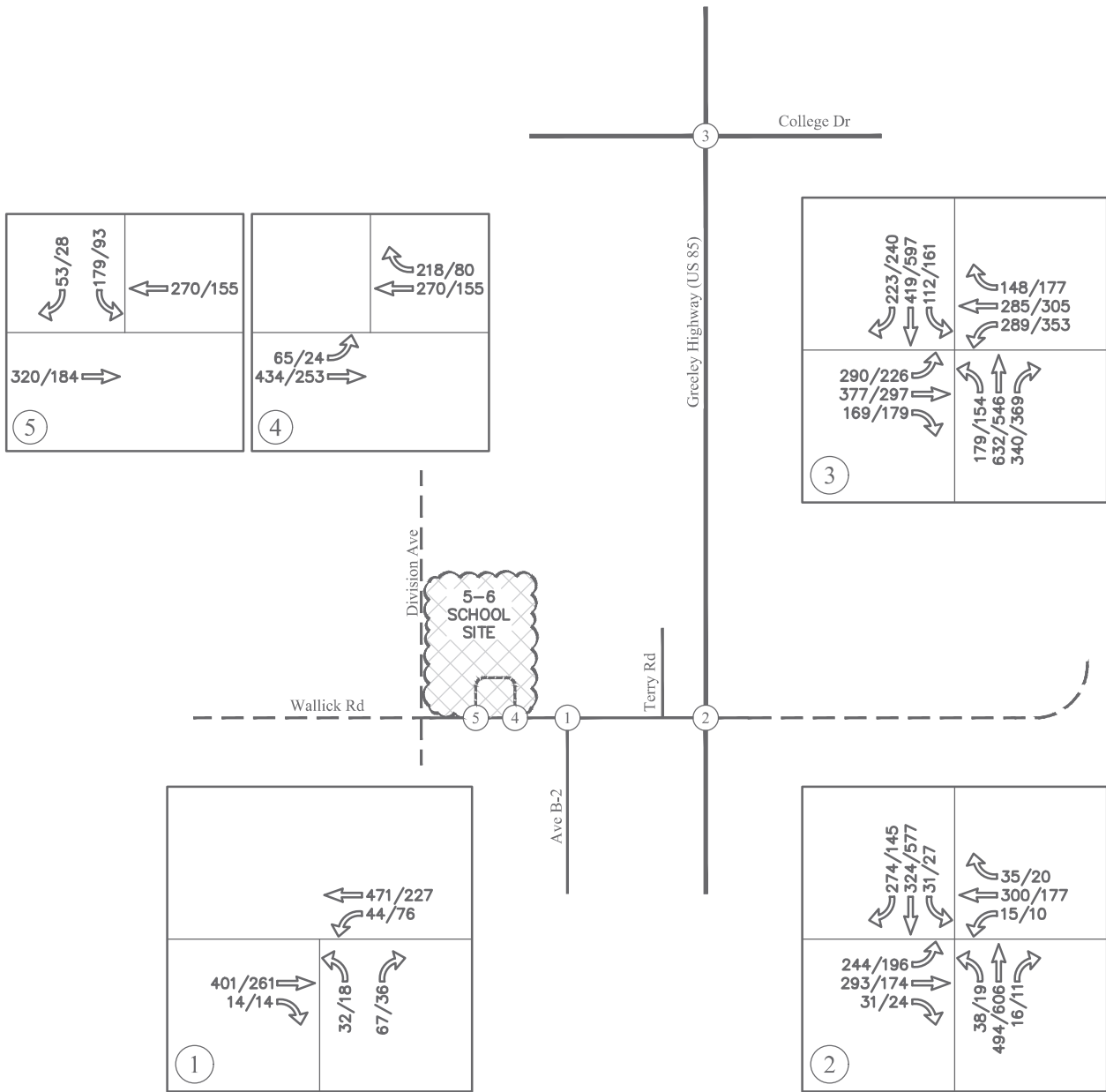
The traffic volumes included in this scenario include the long-range (Year 2050) traffic volumes as shown on Exhibit 12, and the additional traffic from the proposed project, as shown on Exhibit 8. The total traffic volumes anticipated under this condition are shown on Exhibit 13. A summary of the results is shown in Table 10, which follows.

Table 10 – Year 2050 Levels of Service with the Project

INTERSECTION	(S)ignalized (U)nsignalized	Approach Or Overall	AM Peak		Dismissal Peak	
			Delay (sec)	LOS	Delay (sec)	LOS
Avenue B-2 & Wallick Rd	U	NB	17.4	C	12.3	B
S. Greeley Hwy & Wallick Rd	S	Ovr	26.4	C	17.9	B
S. Greeley Hwy & College Dr	S	Ovr	53.9*	D*	34.5	C
Site Egress Driveway & Wallick Rd	U	SB	20.0	C	11.7	B

*one or more movement is shown to operate at LOS F

With the addition of traffic generated by the build out of the proposed project, the study area intersections are mostly anticipated to operate at levels shown in the no-project conditions. The project-related recommendations included in the short-term conditions are anticipated to be sufficient for long-range considerations. The level of service reports for the long-range with the project condition (Year 2050) are provided in the technical appendix.



12/34 – AM Peak Hour/Dismissal Hour



NOT TO SCALE
FEBRUARY 4, 2026

EXHIBIT 13

LONG-RANGE (2050) WITH PROJECT TRAFFIC VOLUMES



CONCLUSIONS & RECOMMENDATIONS

Based upon the analysis, field observations, assumptions, methodologies and results which are provided in the body of this document, this project is anticipated to have impacts to the capacity of the study area intersections that will require mitigation.

Existing Conditions

The existing intersections within the study area are currently operating at acceptable levels except the intersection of S Greeley Highway & Wallick Road, which is operating at LOS E during the dismissal hour. A traffic signal warrant evaluation utilizing the Manual on Uniform Traffic Control Devices (MUTCD) was conducted for this intersection for the existing conditions. The intersection does not meet warrants for a traffic signal at this time.

The posted speed on Wallick Road includes a 20-mph school zone for westbound travel from 6:30 AM to 5:00 PM, but nothing for eastbound travel. The duration of the school zone limitation seems excessive unless the school is commonly utilized by children outside of school hour for playground purposes. Laramie County should review the posted speed limit signage along this roadway and install appropriate speed limit signs for both directions.

Based on the Laramie County Land Use Regulations criteria for speed change lanes, the intersection of Avenue B-2 & Wallick Road has turning volumes that exceed the 10 vph thresholds for a westbound left turn deceleration lane. A left turn lane should be constructed at this intersection. This is included in the Wallick Road improvement plans, which will widen the roadway to a 3-lane section from S Greeley Highway to Division Avenue.

Crash data for the study area intersections was collected for the most recent five years. None of the study area intersections is estimated to have a crash frequency greater than 1.0 crashes per million entering vehicles (MEV), but the intersection of S Greeley Highway & College Drive has a rate of 0.94 crashes per MEV and had a fatality in May 2024. The fatality was a result of a vehicle failing to yield to oncoming traffic, making a left turn from eastbound College Drive. The crash rate near 1.0 and a fatality occurring at this intersection may warrant further investigation for suitable safety improvements at the intersection.

Traffic observations were conducted during the dismissal hour of Afflerbach Elementary on December 18, 2025 by CivTrans Engineering staff. Blockage of eastbound Wallick Road occurs for a couple of minutes as left turning vehicles from Wallick Road wait for the onsite queue to clear. The school does not have sufficient on-site storage for the pick-up operation and many vehicles park along the north and south side of Wallick Road. Wallick Road is posted with “no parking” signs along the north side of the roadway. It is not clear why the north side of Wallick Road is posted with “no parking” signs as there appears to be adequate road width to accommodate the parked vehicles and provide a westbound travel lane. Laramie County should revisit the parking prohibition on this

roadway and either remove the signs or modify them to permit parking during school loading times. The Wallick Road widening project should provide off-site queuing or parking for the westbound direction for Afflerbach Elementary. The addition of an eastbound left turn lane, which is anticipated to be completed as a part of the Wallick Road widening project, should alleviate the blockage issue that currently exists.

Short-term without the Project

The short-term condition without the project is anticipated to operate similar to existing conditions without significant degradation to the level of service at the study area intersections.

Long-range without the Project

With the significant anticipated growth in traffic within the study area over the next 25 years, the added traffic from the extension of Wallick Road, Division Avenue and development in the vicinity, the intersection of S. Greeley Highway & Wallick Road is anticipated to meet warrants for signalization. If the proposed 5/6 school project does not move forward, the intersection of S. Greeley Highway & Wallick Road will require signalization when Wallick Road is extended to the east and/or Division Avenue is connected to Wallick Road.

Additionally, the intersection of S Greeley Highway & College drive is have individual movement deficiencies for the long-range conditions, which are anticipated to be able to be addressed with signal timing modifications.

Short-term with the Project

With the addition of traffic generated by the proposed project, all of the intersections within the study area are anticipated to continue to operate within acceptable levels except the eastbound approach of S. Greeley Highway & Wallick Road, which is anticipated to fall to LOS F during AM peak and dismissal hours with the project traffic added.

A traffic signal warrant evaluation was conducted for the intersection of S. Greeley Highway & Wallick Road for the short-term with project condition. The intersection is anticipated to meet Warrant 2 (four-hour) and Warrant 3 (peak hour) are anticipated to be met with the added traffic from the proposed school. With two of the MUTCD volume warrants for signalization anticipated to be met once the school is constructed and operational, a traffic signal should be considered for installation at this intersection. Installation of a traffic signal is anticipated to alleviate delay and level of service deficiencies at the intersection.

The added school traffic will meet left turn and right turn deceleration lane thresholds along Wallick Road at the ingress-only access to the parking and drop-off/pick-up area. Therefore, these speed change lanes should be installed at this intersection as a part of the project.

Since the egress-only access will be a tee intersection, the school should also consider providing a left turn acceleration lane on Wallick Road at this driveway to provide a safer left turn maneuver for the future once Wallick Road is extended west and carrying higher traffic volume.

The plans for the Cheyenne 5/6 School show three new driveways to Wallick Road. Wallick Road does not have a posted speed limit aside from the school zone associated with Afflerbach Elementary. The 20-mph school zone should be extended to accommodate the new school. The required sight distance along a 20 mph 2-lane roadway is 200' for passenger cars and pickup trucks. These lines of sight should be shown on the construction plans. The resulting sight triangle shall be kept free from obstructions to vision between the heights of 2.5' and 12' above the street grade.

The Cheyenne MPO Safe Routes to School Plan provides recommendations for pedestrian and bicycle infrastructure improvements for the Afflerbach Elementary School. Where achievable, the recommended improvements should be completed to provide safe walking and bicycle infrastructure for non-motorist travelers.

Long-range with the Project

The study area intersections are mostly anticipated to operate at levels shown in the no-project conditions. The project-related recommendations included in the short-term conditions are anticipated to be sufficient for long-range considerations.

**Traffic Impact Study
Cheyenne 5-6 School**

**TECHNICAL APPENDIX
February 4, 2026**

RAW TRAFFIC COUNTS



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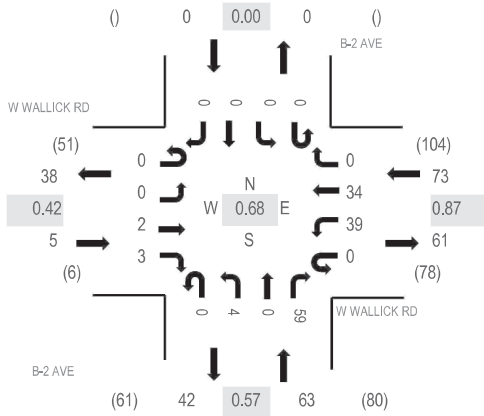
Location: 2 B-2 AVE & W WALLICK RD AM

Date: Wednesday, November 5, 2025

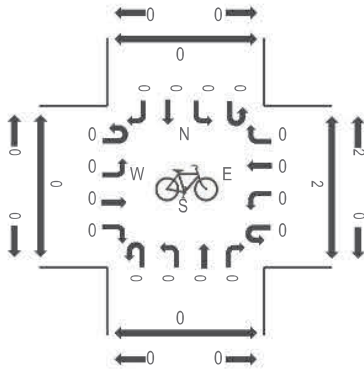
Peak Hour: 07:30 AM - 08:30 AM

Peak 15-Minutes: 08:00 AM - 08:15 AM

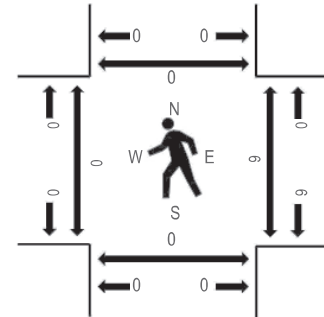
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	W WALLICK RD Eastbound				W WALLICK RD Westbound				B-2 AVE Northbound			B-2 AVE Southbound				Total	Rolling Hour	Pedestrian Crossings				
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru			Right	West	East	South	North
7:00 AM	0	0	0	0	0	0	5	0	0	0	0	2	0	0	0	0	7	65	0	0	0	0
7:15 AM	0	0	1	0	0	2	6	0	0	1	0	4	0	0	0	0	14	110	0	0	0	0
7:30 AM	0	0	0	0	0	2	9	0	0	2	0	2	0	0	0	0	15	141	0	0	0	0
7:45 AM	0	0	0	0	0	6	14	0	0	0	0	9	0	0	0	0	29	140	0	0	0	0
8:00 AM	0	0	1	2	0	16	5	0	0	2	0	26	0	0	0	0	52	125	0	6	0	0
8:15 AM	0	0	1	1	0	15	6	0	0	0	0	22	0	0	0	0	45		0	0	0	0
8:30 AM	0	0	0	0	0	8	1	0	0	0	0	5	0	0	0	0	14		0	0	0	0
8:45 AM	0	0	0	0	0	9	0	0	0	0	0	5	0	0	0	0	14		0	0	0	0
Count Total	0	0	3	3	0	58	46	0	0	5	0	75	0	0	0	0	190		0	6	0	0
Peak Hour	0	0	2	3	0	39	34	0	0	4	0	59	0	0	0	0	141		0	6	0	0



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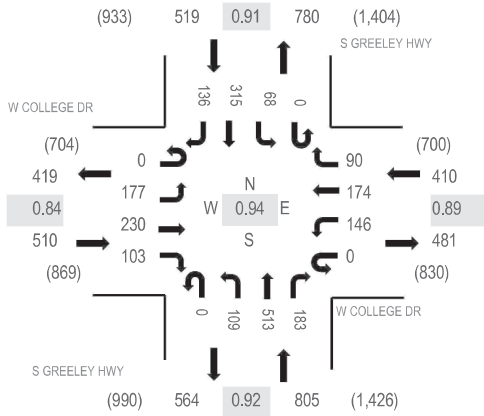
Location: 3 S GREELEY HWY & W COLLEGE DR AM

Date: Wednesday, November 5, 2025

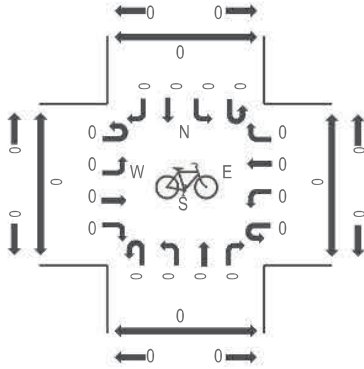
Peak Hour: 07:15 AM - 08:15 AM

Peak 15-Minutes: 07:45 AM - 08:00 AM

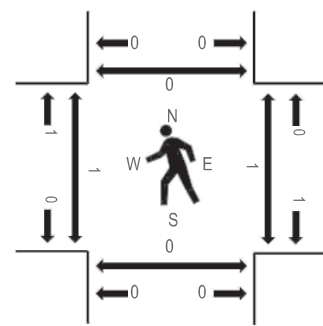
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	W COLLEGE DR Eastbound				W COLLEGE DR Westbound				S GREELEY HWY Northbound				S GREELEY HWY Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
7:00 AM	0	33	39	20	0	35	37	23	0	22	110	34	0	9	55	22	439	2,171	0	0	0	0
7:15 AM	0	39	60	28	0	41	58	19	0	29	132	48	0	10	64	34	562	2,244	0	0	0	0
7:30 AM	0	53	61	29	0	43	42	32	0	28	113	42	0	25	60	47	575	2,122	0	0	0	0
7:45 AM	0	50	73	30	0	32	32	27	0	26	143	50	0	14	88	30	595	1,927	0	1	0	0
8:00 AM	0	35	36	16	0	30	42	12	0	26	125	43	0	19	103	25	512	1,757	1	0	0	0
8:15 AM	0	39	30	18	0	29	26	13	0	19	109	35	0	18	73	31	440		0	0	0	0
8:30 AM	0	42	37	19	0	21	16	17	0	21	82	32	0	21	55	17	380		0	0	0	0
8:45 AM	0	28	35	19	0	30	27	16	0	14	112	31	0	28	52	33	425		0	0	0	0
Count Total	0	319	371	179	0	261	280	159	0	185	926	315	0	144	550	239	3,928		1	1	0	0
Peak Hour	0	177	230	103	0	146	174	90	0	109	513	183	0	68	315	136	2,244		1	1	0	0



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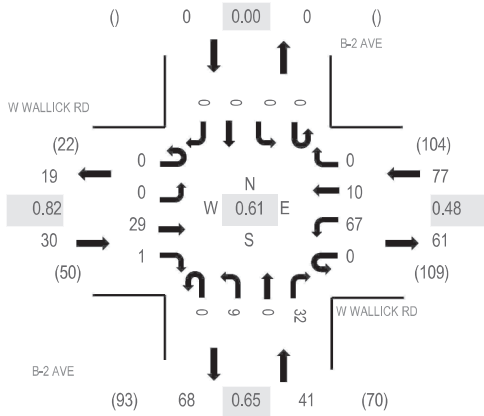
Location: 2 B-2 AVE & W WALLICK RD PM

Date: Wednesday, November 5, 2025

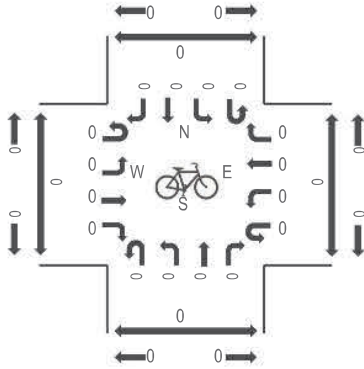
Peak Hour: 03:00 PM - 04:00 PM

Peak 15-Minutes: 03:30 PM - 03:45 PM

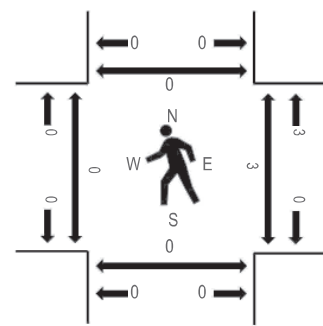
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	W WALLICK RD Eastbound				W WALLICK RD Westbound				B-2 AVE Northbound			B-2 AVE Southbound				Total	Rolling Hour	Pedestrian Crossings				
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru			Right	West	East	South	North
2:30 PM	0	0	1	0	0	6	0	0	0	0	0	6	0	0	0	0	13	87	0	0	0	0
2:45 PM	0	0	1	0	0	6	1	0	0	0	0	8	0	0	0	0	16	135	0	0	1	0
3:00 PM	0	0	0	0	0	12	1	0	0	1	0	11	0	0	0	0	25	148	0	0	0	0
3:15 PM	0	0	2	0	0	11	3	0	0	5	0	12	0	0	0	0	33	148	0	3	0	0
3:30 PM	0	0	13	1	0	36	4	0	0	3	0	4	0	0	0	0	61	137	0	0	0	0
3:45 PM	0	0	14	0	0	8	2	0	0	0	0	5	0	0	0	0	29		0	0	0	0
4:00 PM	0	0	9	0	0	8	2	0	0	0	0	6	0	0	0	0	25		0	0	0	0
4:15 PM	0	0	8	1	0	4	0	0	0	0	0	9	0	0	0	0	22		0	0	0	0
Count Total	0	0	48	2	0	91	13	0	0	9	0	61	0	0	0	0	224		0	3	1	0
Peak Hour	0	0	29	1	0	67	10	0	0	9	0	32	0	0	0	0	148		0	3	0	0

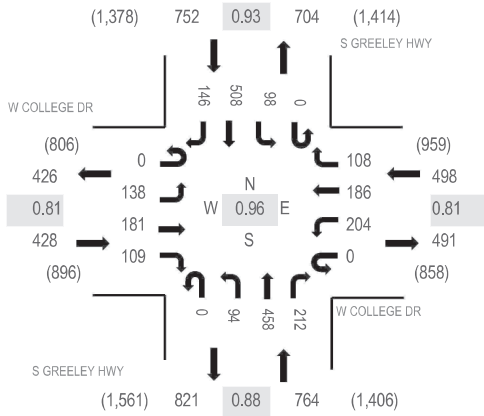
Location: 3 S GREELEY HWY & W COLLEGE DR PM

Date: Wednesday, November 5, 2025

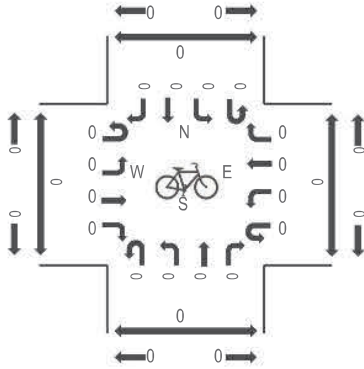
Peak Hour: 03:30 PM - 04:30 PM

Peak 15-Minutes: 04:15 PM - 04:30 PM

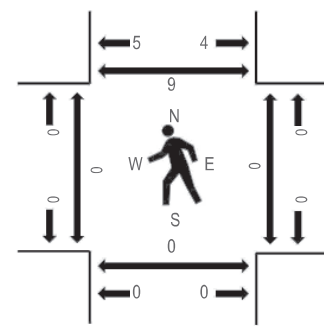
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	W COLLEGE DR Eastbound				W COLLEGE DR Westbound				S GREELEY HWY Northbound				S GREELEY HWY Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
2:30 PM	0	31	38	11	0	40	40	25	0	27	106	28	0	12	96	37	491	2,197	0	1	0	0
2:45 PM	0	46	71	35	0	40	33	20	0	21	95	36	0	13	110	25	545	2,315	0	0	0	0
3:00 PM	0	48	34	35	0	45	33	23	0	14	132	28	0	14	114	39	559	2,384	0	2	0	0
3:15 PM	0	52	40	27	0	71	51	40	0	23	92	40	0	13	116	37	602	2,408	0	2	0	0
3:30 PM	0	35	41	30	0	47	42	28	0	19	136	63	0	15	115	38	609	2,442	0	0	0	0
3:45 PM	0	34	47	27	0	52	41	21	0	24	116	50	0	31	139	32	614		0	0	0	4
4:00 PM	0	40	41	22	0	58	47	27	0	27	87	53	0	23	121	37	583		0	0	0	4
4:15 PM	0	29	52	30	0	47	56	32	0	24	119	46	0	29	133	39	636		0	0	0	1
Count Total	0	315	364	217	0	400	343	216	0	179	883	344	0	150	944	284	4,639		0	5	0	9
Peak Hour	0	138	181	109	0	204	186	108	0	94	458	212	0	98	508	146	2,442		0	0	0	9



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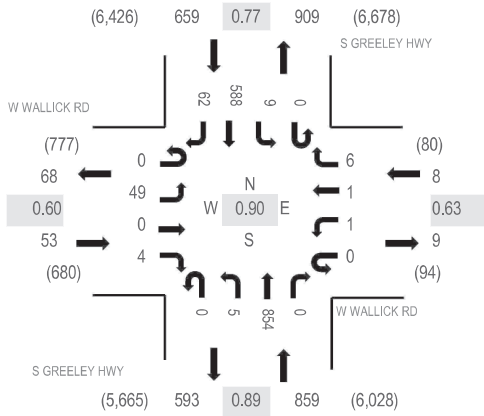
Location: 1 S GREELEY HWY & W WALLICK RD AM

Date: Wednesday, November 5, 2025

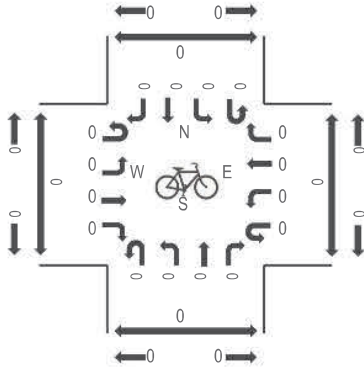
Peak Hour: 04:30 PM - 05:30 PM

Peak 15-Minutes: 05:15 PM - 05:30 PM

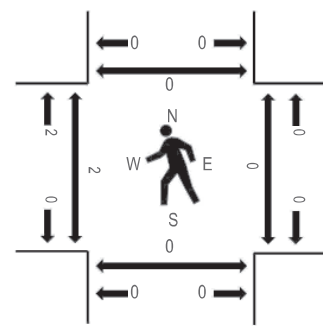
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	W WALLICK RD Eastbound				W WALLICK RD Westbound				S GREELEY HWY Northbound				S GREELEY HWY Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
6:00 AM	0	6	0	2	0	0	0	0	0	0	79	0	0	0	199	0	286	1,177	0	0	0	0
6:15 AM	0	8	0	4	0	0	0	0	0	3	101	0	0	1	224	6	347	1,113	0	0	0	0
6:30 AM	0	15	0	1	0	0	0	0	0	1	125	0	0	1	176	4	323	1,017	0	0	0	0
6:45 AM	0	11	0	2	0	0	0	0	0	0	110	0	0	0	95	3	221	911	0	0	0	0
7:00 AM	0	9	1	1	0	0	0	1	0	1	134	0	0	1	67	7	222	952	0	0	0	0
7:15 AM	0	8	0	0	0	0	0	3	0	1	140	0	0	0	90	9	251	986	0	0	0	0
7:30 AM	0	11	0	0	0	0	0	1	0	2	120	0	0	0	72	11	217	961	0	0	0	0
7:45 AM	0	10	1	0	0	0	0	1	0	2	127	1	0	0	84	36	262	926	0	0	0	0
8:00 AM	0	47	1	3	0	0	0	2	0	3	109	0	0	1	60	30	256	831	0	0	0	0
8:15 AM	0	35	1	2	0	0	1	0	0	3	80	0	0	0	70	34	226	747	0	0	0	0
8:30 AM	0	23	0	0	0	0	0	0	0	2	91	1	0	1	58	6	182	688	0	0	0	0
8:45 AM	0	9	0	0	0	0	0	0	0	0	91	0	0	1	54	12	167	662	0	0	0	0
9:00 AM	0	6	0	0	0	0	0	1	0	0	87	0	0	1	75	2	172	655	0	0	0	0
9:15 AM	0	9	1	0	0	0	0	2	0	0	89	1	0	0	63	2	167	651	0	0	0	0
9:30 AM	0	5	0	1	0	0	1	1	0	1	74	0	0	0	73	0	156	637	0	0	0	0
9:45 AM	0	4	0	0	0	0	0	0	0	0	71	0	0	0	85	0	160	663	0	0	0	0
10:00 AM	0	6	0	1	0	0	0	2	0	0	96	0	0	2	55	6	168	648	0	0	0	0
10:15 AM	0	3	0	0	0	0	0	0	0	0	85	0	0	0	58	7	153	653	0	0	0	0
10:30 AM	0	5	0	1	0	0	0	1	0	1	86	0	0	5	77	6	182	672	0	0	0	0
10:45 AM	0	6	0	0	0	0	0	4	0	0	70	0	0	1	53	11	145	707	0	0	0	0
11:00 AM	0	5	0	0	0	0	0	1	0	0	95	0	0	0	63	9	173	742	0	0	0	0
11:15 AM	0	9	0	1	0	0	0	1	0	0	95	0	0	0	60	6	172	830	0	0	0	0
11:30 AM	0	5	0	0	0	0	0	0	0	0	115	0	0	1	91	5	217	862	0	0	0	0
11:45 AM	0	9	0	1	0	0	0	3	0	0	88	0	0	0	73	6	180	845	0	0	0	0
12:00 PM	0	3	0	0	0	0	0	2	0	2	121	1	0	1	121	10	261	856	0	0	0	0
12:15 PM	0	3	0	0	0	0	0	3	0	0	80	0	0	2	101	15	204	807	0	0	0	0
12:30 PM	0	4	0	1	0	0	0	2	0	0	81	0	0	2	97	13	200	794	0	0	0	0
12:45 PM	0	13	0	0	0	0	0	1	0	1	72	0	0	1	90	13	191	787	0	0	0	0
1:00 PM	0	8	0	0	0	0	0	1	0	0	96	0	0	3	93	11	212	785	0	0	0	0
1:15 PM	0	9	0	0	0	0	0	3	0	0	76	0	0	1	97	5	191	762	0	0	0	0
1:30 PM	0	7	0	1	0	0	0	1	0	1	77	0	0	3	93	10	193	781	0	0	0	0
1:45 PM	0	11	0	1	0	0	0	4	0	0	66	0	0	1	99	7	189	813	0	0	0	0
2:00 PM	0	5	0	0	0	0	0	1	0	0	74	0	0	1	99	9	189	840	0	0	0	0
2:15 PM	0	5	0	1	0	0	0	0	0	0	91	0	0	0	100	13	210	951	0	0	0	0

2:30 PM	0	10	0	0	0	0	0	1	0	1	104	0	0	2	95	12	225	1,048	0	0	0	0
2:45 PM	0	10	0	0	0	0	0	3	0	0	74	0	0	1	106	22	216	1,166	0	0	0	0
3:00 PM	0	9	0	1	0	0	0	3	0	2	129	0	0	5	111	40	300	1,265	0	0	0	0
3:15 PM	0	12	0	1	0	0	0	1	0	5	123	0	0	3	129	33	307	1,254	0	0	0	0
3:30 PM	0	41	0	3	0	0	0	3	0	1	153	0	0	3	106	33	343	1,256	0	0	0	0
3:45 PM	0	30	3	3	0	0	0	1	0	0	126	0	0	0	136	16	315	1,260	0	0	0	0
4:00 PM	0	17	0	2	0	0	0	3	0	0	121	0	0	1	128	17	289	1,347	0	0	0	0
4:15 PM	0	22	0	0	0	0	0	1	0	3	135	1	0	2	139	6	309	1,450	0	0	0	0
4:30 PM	0	15	0	1	0	0	0	0	0	0	166	0	0	0	141	24	347	1,579	0	0	0	0
4:45 PM	0	19	0	2	0	0	0	3	0	3	234	0	0	4	131	6	402	1,571	0	0	0	0
5:00 PM	0	4	0	1	0	0	0	1	0	2	206	0	0	2	158	18	392	1,415	2	0	0	0
5:15 PM	0	11	0	0	0	1	1	2	0	0	248	0	0	3	158	14	438	1,232	0	0	0	0
5:30 PM	0	6	1	1	0	0	0	2	0	0	193	0	0	0	123	13	339	972	0	0	0	0
5:45 PM	0	8	0	0	0	0	0	2	0	1	110	0	0	2	104	19	246	776	1	0	0	0
6:00 PM	0	15	0	1	0	0	0	0	0	0	97	1	0	2	80	13	209	693	0	0	0	0
6:15 PM	0	5	0	0	0	0	0	0	0	0	80	0	0	1	84	8	178	608	0	0	0	0
6:30 PM	0	4	0	1	0	0	0	1	0	1	61	0	0	2	62	11	143	551	0	0	0	0
6:45 PM	0	10	0	0	0	0	0	1	0	0	67	0	0	0	73	12	163	491	0	0	0	0
7:00 PM	0	5	0	0	0	0	0	0	0	1	46	0	1	0	56	15	124	422	0	0	0	0
7:15 PM	0	4	0	1	0	0	0	1	0	0	42	0	0	2	66	5	121	406	0	0	0	0
7:30 PM	0	5	0	0	0	0	0	1	0	0	31	0	0	2	38	6	83	385	0	0	0	0
7:45 PM	0	5	0	0	0	0	0	0	0	1	40	0	0	1	38	9	94	390	0	0	0	0
8:00 PM	0	3	0	1	0	0	0	1	1	0	41	0	0	0	49	12	108	360	0	0	0	0
8:15 PM	0	5	0	0	0	0	0	1	0	0	32	0	0	3	49	10	100	315	0	0	0	0
8:30 PM	0	3	0	0	0	0	0	1	0	1	28	0	0	4	47	4	88	279	0	0	0	0
8:45 PM	0	5	0	0	0	0	0	0	0	0	22	0	0	0	33	4	64	249	0	0	0	0
9:00 PM	0	3	0	0	0	0	0	0	0	0	19	0	0	1	33	7	63	226	0	0	0	0
9:15 PM	0	5	0	1	0	0	0	1	0	0	16	0	0	1	34	6	64		0	0	0	0
9:30 PM	0	3	0	0	0	0	0	0	0	0	25	0	0	0	25	5	58		0	0	0	0
9:45 PM	0	1	0	0	0	0	0	0	0	1	13	0	0	1	22	3	41		0	0	0	0
Count Total	0	627	9	44	0	1	3	76	1	47	5,974	6	1	79	5,619	727	13,214		3	0	0	0
Peak Hour	0	49	0	4	0	1	1	6	0	5	854	0	0	9	588	62	1,579		2	0	0	0

TRIP GENERATION CALCULATIONS

Trip Generation for Proposed 5-6 School (Cheyenne, WY)

The current project is proposing to build a new school that will accommodate grades 5 and 6 in the South Triad of the Laramie County School District near Cheyenne, Wyoming. The school is anticipated to have a capacity of 650 students and will operate with an initial enrollment of 632 students (2024 value). Per the 12th edition of ITE's *Trip Generation*, this proposed use falls within land use category (LUC), 522 "Middle School / Junior High School."

Approximately 90 of the grade 5/6 students currently attend Afflerbach Elementary and are already creating trips to the area as Afflerbach is adjacent to this site. Therefore, the net increase in students to this area from the proposed 5/6 school is estimated to be 560 students (at capacity).

Students within 1/2 mile of the school are anticipated to walk or bicycle and are not eligible for bus drop-off / pick-up. The South Triad currently is above average for bus usage. Current student addresses show approximately 12 residences within 1/2 mile of the school, which makes up <5% of the current residences that would attend this school.

Trip Generation

Per Trip Generation Manual, the following land use category was applied.

<u>LUC 522 - Middle / Junior High School</u>	Vehicle						
	Fitted Curve Equation:	Trip Rate:	R ²	% in	%out	# Studies	Std Dev.
AM Peak:	LN(T)=0.93*LN(X)+0.22	0.75	0.82	55%	45%	27	0.16
PM Peak:	T=0.34(X)+4.16	0.34	0.64	46%	54%	39	0.64
ADT:	LN(T)=0.97*LN(X)+0.97	2.09	0.79	50%	50%	13	0.43

		<u>Vehicular Trips</u>		
		<u>Total</u>	<u>In</u>	<u>Out</u>
For X=	560	AM Peak= 448	246	202
		PM Peak= 195	90	105
		ADT= 1,222		

Equation used for AM, dismissal peak and ADT as there are 20+ studies or R²>0.75 and SD<0.55

		<u>Vehicular Trips</u>		
		<u>Total</u>	<u>In</u>	<u>Out</u>
For X=	650	AM Peak= 515	283	232
		PM Peak= 225	104	121
		ADT= 1,412		

Equation used for AM peak and ADT. Rate used for PM peak as R² too low.

CRASH DATA

INTERSECTION CRASH HISTORY REPORT FOR W WALLICK RD & AVE B-2 (ID 1022893) FOR THE YEARS 2020 - 2024

DATE	TIME	ID #	CRASH STREET	CROSS STREET	JUNCTION RELATION	INTERSECTION TYPE	MANNER OF COLLISION	CRASH SEVERITY	FATAL INJURY	FIRST HARMFUL EVENT (FHE)	FHE LOCATION #	DIRECTION	VEHICLE MANEUVER	DRIVER ACTION	PERSON TYPE	SAFETY EQUIPMENT USED	INJURY STATUS	LIGHTING	WEATHER CONDITION	ROAD CONDITION	ALCOHOL INVOLVED	DRUGS INVOLVED	LATITUDE	LONGITUDE	CRASH CASE #	
2024	04/26/2024	1812	1022893	CR 461 / W WALLICK RD	AVE B-2	Intersection	T Intersection	Not a Collision w/2 Vehicles in Transport	0	1	Pedestrian	1	West	Turning Left	1	No Improper Driving	Driver	Shoulder and Lap Belt	No Apparent Injury	Clear	Dry	N	N	-104.804845	-24.415879	24-015879
								SUSPECTED MINOR INJURY							1	Passenger	Shoulder and Lap Belt	No Apparent Injury								
															1	Passenger	Shoulder and Lap Belt	No Apparent Injury								
															1	Pedestrian		Suspected Minor Injury								

REPORT SUMMARY	
TOTAL CRASHES:	1
FATAL CRASHES:	0
INJURY CRASHES:	1
PDO CRASHES:	0
TOTAL FATALITIES:	0
TOTAL INJURIES:	1

YEAR	CRASH COUNTS			SEVERITY COUNTS			INJURY COUNTS			
	FATAL CRASHES	INJURY CRASHES	PDO CRASHES	CRITICAL CRASHES	SERIOUS CRASHES	DAMAGE CRASHES	FATAL INJURIES	SERIOUS INJURIES	MINOR INJURIES	POSSIBLE INJURIES
2024	0	1	0	0	1	0	0	0	1	0

- * PDO = Property Damage Only Crashes
- * Critical Crashes = Fatal & Suspected Serious Injury Crashes
- * Serious Crashes = Suspected Minor Injury & Possible Injury Crashes
- * Damage Crashes = No Apparent Injury & Unknown Injury Crashes

INTERSECTION CRASH HISTORY REPORT FOR WALLICK RD & S GREELEY HWY (ID 11097) FOR THE YEARS 2020 - 2024

DATE	TIME	ID #	CRASH STREET	CROSS STREET	JUNCTION RELATION	INTERSECTION TYPE	MANNER OF COLLISION	CRASH SEVERITY	FATAL INJURY	FIRST HARMFUL EVENT (FHE)	FHE LOCATION	VEHICLE DIRECTION	MANEUVER	DRIVER ACTION	PERSON TYPE	SAFETY EQUIPMENT USED	INJURY STATUS	LIGHTING	WEATHER CONDITION	ROAD CONDITION	ALCOHOL INVOLVED	DRUGS INVOLVED	CRASH CASE #
2020																							
02/07/2020	835	11097	S GREELEY HWY US 85 BUS		Driveway Related	Not an Intersection	Angle (Front to Side), Opposing Direction	SUSPECTED MINOR INJURY	0	2	Motor Vehicle in Transport on Roadway	1 East 2 South	1 Turning Left 2 Straight Ahead	1 Disregarded Traffic Signs 2 No Improper Driving	1 Driver 2 Driver	Shoulder and Lap Belt Shoulder and Lap Belt	No Apparent Injury Suspected Injury	Daylight	Clear	Dry	N	N	P2020017534
05/07/2020	1712	11097	S GREELEY HWY US 85 BUS		Intersection Related	Four (4)-Way Intersection	Rear End (Front to Rear)	SUSPECTED SERIOUS INJURY	0	2	Motor Vehicle in Transport on Roadway	1 South 2 South	1 Stopped in Traffic 2 Straight Ahead	1 No Improper Driving 2 Failed to Keep Proper Lane Spacing	1 Driver 2 Passenger	Shoulder and Lap Belt Shoulder and Lap Belt	Minor Injury Suspected Injury	Daylight	Clear	Dry	Y	N	P2020017534
2021																							
05/14/2021	1550	11097	S GREELEY HWY US 85 BUS	W WALLICK RD CR 461	Intersection	Four (4)-Way Intersection	Angle Right (Front to Side, Includes Broadside)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	1 East 2 South	1 Turning Left 2 Straight Ahead	1 Failed to Yield ROW 2 No Improper Driving	1 Driver 2 Driver	Shoulder and Lap Belt Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	Dry	N	N	21-0047272
2022																							
02/28/2022	1546	11097	S GREELEY HWY US 85 BUS	CR 461 W WALLICK RD	Intersection	T Intersection	Angle Right (Front to Side, Includes Broadside)	POSSIBLE INJURY	0	2	Motor Vehicle in Transport on Roadway	1 South 2 East	1 Straight Ahead 2 Turning Left	1 No Improper Driving 2 Failed to Yield ROW	1 Driver 1 Passenger	Shoulder and Lap Belt Shoulder and Lap Belt	Possible Injury Possible Injury	Daylight	Clear	Dry	N	N	22-0022029
11/14/2022	885	11097	S GREELEY HWY US 85 BUS	CR 461 W WALLICK RD	Intersection	Four (4)-Way Intersection	Angle (Front to Side), Opposing Direction	POSSIBLE INJURY	0	1	Motor Vehicle in Transport on Roadway	1 East 2 South	1 Turning Left 2 Straight Ahead	1 Failed to Yield ROW 2 No Improper Driving	1 Driver 2 Driver	Shoulder and Lap Belt Shoulder and Lap Belt	Possible Injury No Apparent Injury	Daylight	Clear	Dry	N	N	22-00120227
2023																							
12/07/2023	1600	11097	CR 461 W WALLICK RD		Intersection Related	Four (4)-Way Intersection	Rear End (Front to Rear)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	1 East 2 East	1 Straight Ahead 2 Other	1 Following too Close 2 No Improper Driving	1 Driver 2 Driver	Unknown Shoulder and Lap Belt	No Apparent Injury No Apparent Injury	Dusk	Clear	Dry	N	N	23-29722

DATE TIME ID # CRASH STREET CROSS STREET JUNCTION INTERSECTION TYPE MANNER OF COLLISION CRASH SEVERITY FATAL INJURY CRASHES FATAL INJURY CRASHES FIRST HARMFUL EVENT (FHE) FHE LOCATION # VEHICLE DIRECTION # VEHICLE MANEUVER DRIVER ACTION PERSON TYPE SAFETY EQUIPMENT USED INJURY STATUS LIGHTING WEATHER CONDITION ROAD CONDITION INVOLVED ALCOHOL DRUGS LATITUDE LONGITUDE CRASH CASE #

REPORT SUMMARY

TOTAL CRASHES: 6
 FATAL CRASHES: 0 TOTAL FATALITIES: 0
 INJURY CRASHES: 4 TOTAL INJURIES: 7
 PDO CRASHES: 2

YEAR	CRASH COUNTS			SEVERITY COUNTS			INJURY COUNTS					
	FATAL CRASHES	INJURY CRASHES	PDO CRASHES	TOTAL CRASHES	CRITICAL CRASHES	SERIOUS CRASHES	DAMAGE CRASHES	FATAL INJURIES	SERIOUS INJURIES	MINOR INJURIES	POSSIBLE INJURIES	NO INJURIES
2020	0	2	0	2	1	1	0	0	1	3	0	3
2021	0	0	1	1	0	0	1	0	0	0	0	2
2022	0	2	0	2	0	2	0	0	0	0	3	7
2023	0	0	1	1	0	0	1	0	0	0	0	2
	0	4	2	6	1	3	2	0	1	3	3	14

* PDO = Property Damage Only Crashes
 * Critical Crashes = Fatal & Suspected Serious Injury Crashes
 * Serious Crashes = Suspected Minor Injury & Possible Injury Crashes
 * Damage Crashes = No Apparent Injury & Unknown Injury Crashes

INTERSECTION CRASH HISTORY REPORT FOR COLLEGE DR & S GREELEY HWY (ID 11092) FOR THE YEARS 2020 - 2024

DATE	TIME	ID #	CROSS STREET	CROSS STREET	JUNCTION RELATION	INTERSECTION TYPE	MANNER OF COLLISION	CRASH SEVERITY	FATAL INJURY	FIRST HARMFUL EVENT (FHE)	LOCATION	#	DIRECTION	VEHICLE MANEUVER	DRIVER ACTION	PERSON TYPE	SAFETY EQUIPMENT USED	INJURY STATUS	LIGHTING	WEATHER CONDITION	ROAD CONDITION	ALCOHOL INVOLVED	DRUGS INVOLVED	CRASH CASE #
03/01/2020	1830	11092	S GREELEY HWY US 85 BUS	S GREELEY HWY US 85 BUS	Intersection Related	Four (4)-Way Intersection	Rear End (Front to Rear)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	1	North	Straight Ahead	1 Following too Close	1 Driver	Shoulder and Lap Belt	No Apparent Injury	Darkest Lighted	Snowing	Snow	N	N	41.102312 -104.800517 P2020020597
											2	North	Slowing	2 No Improper Driving	2 Driver	Shoulder and Lap Belt	No Apparent Injury			Ice/Frost				
																2 Passenger	Enhanced	No Apparent Injury						
																2 Passenger	Facing Child	No Apparent Injury						
																2 Passenger	Rear Facing	No Apparent Injury						
																2 Passenger	Child Restraint	No Apparent Injury						
07/04/2020	2010	11092	S GREELEY HWY US 85 BUS	S GREELEY HWY US 85 BUS	Intersection Related	Four (4)-Way Intersection	Angle (Front to Side, Opposing Direction)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	1	North	Turning Left	1 No Improper Driving	1 Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	Dry	N	N	41.102443 -104.800515 P2020085391
											2	South	Stopped in Traffic	2 Disregarded Other Road Rule/Sign	1 Passenger	Shoulder and Lap Belt	No Apparent Injury							
																2 Driver	Shoulder and Lap Belt	No Apparent Injury						
																2 Passenger	Booster Seat	No Apparent Injury						
																2 Passenger	Shoulder and Lap Belt	No Apparent Injury						
																2 Passenger	Shoulder and Lap Belt	No Apparent Injury						
07/24/2020	2005	11092	S GREELEY HWY US 85 BUS	S GREELEY HWY US 85 BUS	Intersection Related	Four (4)-Way Intersection	Angle Right (Front to Side, Opposing Direction)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	1	East	Straight Ahead	1 No Improper Driving	1 Driver	Shoulder and Lap Belt	No Apparent Injury	Dusk	Clear	Dry	N	N	41.102318 -104.800476 P2020095514
											2	North	Straight Ahead	2 Run Red Light	2 Driver	Lap Belt	No Apparent Injury							
																2 Driver	None Used	No Apparent Injury						
																2 Driver	Shoulder and Lap Belt	No Apparent Injury						
																2 Driver	Shoulder and Lap Belt	Possible Injury						
08/09/2020	1638	11092	S GREELEY HWY US 85 BUS	S GREELEY HWY US 85 BUS	Intersection Related	Four (4)-Way Intersection	Rear End (Front to Rear)	POSSIBLE INJURY	0	1	Motor Vehicle in Transport on Roadway	1	East	Straight Ahead	1 Following too Close	1 Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	Dry	N	N	41.102336 -104.800790 P202014759
																2 Driver	Shoulder and Lap Belt	No Apparent Injury						
																2 Driver	Shoulder and Lap Belt	No Apparent Injury						
																2 Driver	Shoulder and Lap Belt	No Apparent Injury						
09/28/2020	1648	11092	S GREELEY HWY US 85 BUS	S GREELEY HWY US 85 BUS	Intersection Related	Four (4)-Way Intersection	Angle (Front to Side, Opposing Direction)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	1	West	Turning Left	1 Failed to Yield ROW	1 Driver	Shoulder and Lap Belt	No Apparent Injury	Dusk	Clear	Dry	N	N	41.102280 -104.800097 P2020143366
																2 Driver	Shoulder and Lap Belt	No Apparent Injury						
																2 Driver	Shoulder and Lap Belt	No Apparent Injury						
																2 Driver	Shoulder and Lap Belt	No Apparent Injury						
10/22/2020	1345	11092	S GREELEY HWY US 85 BUS	S GREELEY HWY US 85 BUS	Intersection Related	Four (4)-Way Intersection	Angle Right (Front to Side, Opposing Direction)	SUSPECTED MINOR INJURY	0	1	Motor Vehicle in Transport on Roadway	1	North	Straight Ahead	1 Ran Red Light	1 Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	Dry	N	N	41.102302 -104.800514 P2020144477
																2 Driver	Shoulder and Lap Belt	Suspected Injury						
																2 Driver	Shoulder and Lap Belt	No Apparent Injury						
																2 Driver	Shoulder and Lap Belt	Minor Injury						
10/21/2020	423	11092	S GREELEY HWY US 85 BUS	S GREELEY HWY US 85 BUS	Intersection Related	Four (4)-Way Intersection	Not a Collision w/ Vehicles in Transport	PROPERTY DAMAGE ONLY	0	0	Raised Median or Cub	1	North	Turning Right	1 Erratic/Reckless/Callous Aggressive Ran On Road	1 Driver	Shoulder and Lap Belt	No Apparent Injury	Darkest Lighted	Raining	Dry	N	N	41.102219 -104.800078 P202008
																1 Driver	Shoulder and Lap Belt	No Apparent Injury						
																1 Driver	Shoulder and Lap Belt	No Apparent Injury						
																1 Driver	Shoulder and Lap Belt	No Apparent Injury						
10/25/2020	1821	11092	S GREELEY HWY US 85 BUS	S GREELEY HWY US 85 BUS	Intersection Related	Four (4)-Way Intersection	Rear End (Front to Rear) (Normally Backing)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	1	West	Backing	1 Proper Lane Change	1 Driver	Shoulder and Lap Belt	No Apparent Injury	Darkest Lighted	Snowing	Ice/Frost	N	N	41.102307 -104.800866 P2020152861
																2 Passenger	Shoulder and Lap Belt	No Apparent Injury						
																2 Driver	Shoulder and Lap Belt	No Apparent Injury						
																2 Driver	Shoulder and Lap Belt	No Apparent Injury						
																2 Driver	Shoulder and Lap Belt	No Apparent Injury						
11/08/2020	1607	11092	S GREELEY HWY US 85 BUS	S GREELEY HWY US 85 BUS	Intersection Related	Four (4)-Way Intersection	Rear End (Front to Rear)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	1	North	Stopped in Traffic	1 No Improper Driving	1 Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Snowing	Snow	N	N	41.102160 -104.800512 P2020157877
																2 Driver	Shoulder and Lap Belt	No Apparent Injury						
																2 Driver	Shoulder and Lap Belt	No Apparent Injury						

DATE	TIME	ID #	CRASH STREET	CROSS STREET	JUNCTION RELATION	INTERSECTION TYPE	MANNER OF COLLISION	CRASH SEVERITY	FATAL INJURY	FIRST HARMFUL EVENT (FHE)	PHE LOCATION	#	DIRECTION	VEHICLE MANEUVER	DRIVER ACTION	PERSON TYPE	SAFETY EQUIPMENT USED	INJURY STATUS	LIGHTING	WEATHER CONDITION	ROAD CONDITION	ALCOHOL INVOLVED	DRUGS INVOLVED	CRASH CASE #	
2021																									
02/19/2021	2:25	11092	S GREELEY HWY 25 BUS US 85 BUS	25 BUS US 85 BUS	Intersection	Four (4-Way) Intersection	Angle Right (Front to Side) (Broadside)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	1	South	Straight Ahead	1	No Improper Driving	1	Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	No	No	41.102343 -104.800680 21-40014196
04/20/2021	18:10	11092	S GREELEY HWY 25 BUS US 85 BUS	25 BUS US 85 BUS	Intersection	Four (4-Way) Intersection	Head On (Front to Front)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	2	West	Straight Ahead	2	Ran Red Light	2	Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	No	No	41.102343 -104.800680 21-40014196
04/20/2021	18:10	11092	S GREELEY HWY 25 BUS US 85 BUS	25 BUS US 85 BUS	Intersection	Four (4-Way) Intersection	Head On (Front to Front)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	2	West	Straight Ahead	2	Ran Red Light	2	Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	No	No	41.102343 -104.800680 21-40014196
04/20/2021	7:55	11092	S GREELEY HWY 25 BUS US 85 BUS	25 BUS US 85 BUS	Intersection	Four (4-Way) Intersection	Rear End (Front to Rear)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	1	North	Turning Right	1	No Improper Driving	1	Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	No	No	41.102343 -104.800680 21-40014196
04/20/2021	7:55	11092	S GREELEY HWY 25 BUS US 85 BUS	25 BUS US 85 BUS	Intersection	Four (4-Way) Intersection	Rear End (Front to Rear)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	2	North	Turning Right	2	No Improper Driving	2	Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	No	No	41.102343 -104.800680 21-40014196
07/29/2021	17:09	11092	S GREELEY HWY 25 BUS US 85 BUS	25 BUS US 85 BUS	Intersection	Four (4-Way) Intersection	Head On (Front to Front)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	1	West	Turning Left	1	No Improper Driving	1	Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	No	No	41.102321 -104.800514 21-40086842
11/07/2021	18:15	11092	S GREELEY HWY 25 BUS US 85 BUS	25 BUS US 85 BUS	Intersection	Four (4-Way) Intersection	Not a Collision w/2 Vehicles in Transport	SUSPECTED MINOR INJURY	0	1	Pedestrian	1	West	Straight Ahead	1	No Improper Driving	1	Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	No	No	41.102301 -104.800733 21-40128626
12/09/2021	15	11092	S GREELEY HWY 25 BUS US 85 BUS	25 BUS US 85 BUS	Intersection	Four (4-Way) Intersection	Angle Right (Front to Side) (Broadside)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	2	South	Straight Ahead	2	Ran Red Light	2	Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	No	No	41.102343 -104.800546 21-40193909
12/11/2021	8:09	11092	S GREELEY HWY 25 BUS US 85 BUS	25 BUS US 85 BUS	Intersection	Four (4-Way) Intersection	Not a Collision w/2 Vehicles in Transport	SUSPECTED MINOR INJURY	0	1	Pedestrian	1	South	Turning Left	1	Failed to Yield ROW	1	Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	No	No	41.102320 -104.800587 21-40164063
2022																									
01/11/2022	11:00	11092	S GREELEY HWY 25 BUS US 85 BUS	25 BUS US 85 BUS	Intersection	Four (4-Way) Intersection	Angle Same Direction (Front to Side)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	1	South	Straight Ahead	1	Unknown	1	Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	No	No	41.102389 -104.800648 22-00003290
01/24/2022	22:30	11092	S GREELEY HWY 25 BUS US 85 BUS	25 BUS US 85 BUS	Intersection	Four (4-Way) Intersection	Angle (Front to Side) Opposing Direction	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	1	East	Straight Ahead	1	No Improper Driving	1	Driver	Shoulder and Lap Belt	No Apparent Injury	Darkness Lighted	Clear	No	No	41.102321 -104.800516 22-00008248
03/23/2022	1:25	11092	S GREELEY HWY 25 BUS US 85 BUS	25 BUS US 85 BUS	Intersection	Four (4-Way) Intersection	Angle Right (Front to Side) (Broadside)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	1	North	Turning Left	1	Failed to Yield ROW	1	Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	No	No	41.102322 -104.800519 22-00009246
04/11/2022	15:23	11092	S GREELEY HWY 25 BUS US 85 BUS	25 BUS US 85 BUS	Intersection	Four (4-Way) Intersection	Angle (Front to Side), Opposing Direction	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	2	South	Straight Ahead	2	No Improper Driving	2	Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	No	No	41.102320 -104.800511 22-00009732

DATE	TIME	ID #	CROSS STREET	CROSS STREET	JUNCTION RELATION	INTERSECTION TYPE	MANNER OF COLLISION	CRASH SEVERITY	FATAL INJURY	FIRST HARMFUL EVENT (FHE)	PHE LOCATION	#	DIRECTION	VEHICLE MANEUVER	DRIVER ACTION	PERSON TYPE	SAFETY EQUIPMENT USED	INJURY STATUS	LIGHTING	WEATHER CONDITION	ROAD CONDITION	ALCOHOL INVOLVED	DRUGS INVOLVED	CRASH CASE #		
05/03/2022	937	11092	S GREELEY HWY 25 BUS / US 85 BUS	COLLEGE DR WY 212	Intersection	Four (4)-Way Intersection	Angle (Front to Side), Opposing Direction	POSSIBLE INJURY	0	1	Motor Vehicle in Transport on Roadway	On Roadway	1	East	Straight Ahead	1 No Improper Driving	1 Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	Dry	N	N	2022-31195	
05/04/2022	1107	11092	S GREELEY HWY 25 BUS / US 85 BUS	COLLEGE DR WY 212	Intersection	Four (4)-Way Intersection	Angle Right (Front to Side), Opposing Direction (Broadside)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	On Roadway	1	South	Straight Ahead	1 No Improper Driving	1 Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	Dry	N	N	22-00092823	
07/30/2022	1200	11092	S GREELEY HWY 25 BUS / US 85 BUS	COLLEGE DR WY 212	Intersection	Four (4)-Way Intersection	Angle Same Direction (Front to Side)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	On Roadway	1	West	Straight Ahead	1 Ran Red Light	1 Driver	Lap Belt	No Apparent Injury	Daylight	Clear	Dry	N	N	22-00081638	
11/02/2022	1130	11092	S GREELEY HWY 25 BUS / US 85 BUS	COLLEGE DR WY 212	Intersection	Four (4)-Way Intersection	Angle Right (Front to Side), Opposing Direction (Broadside)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	On Roadway	1	North	Straight Ahead	1 Disregarded Traffic Signs	1 Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	Dry	N	N	22-00117189	
12/22/2022	2045	11092	S GREELEY HWY 25 BUS / US 85 BUS	COLLEGE DR WY 212	Intersection	Four (4)-Way Intersection	Angle Right (Front to Side), Opposing Direction (Broadside)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	On Roadway	1	East	Straight Ahead	1 No Improper Driving	1 Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	Isofrost	N	N	22-00192479	
2023																										
03/11/2023	1740	11092	S GREELEY HWY 25 BUS / US 85 BUS	COLLEGE DR WY 212	Intersection	Four (4)-Way Intersection	Angle (Front to Side), Opposing Direction	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	On Roadway	1	East	Straight Ahead	1 No Improper Driving	1 Driver	Shoulder and Lap Belt	No Apparent Injury	Down	Clear	Dry	N	N	23-00027186	
03/17/2023	2123	11092	S GREELEY HWY 25 BUS / US 85 BUS	COLLEGE DR WY 212	Intersection Related	Four (4)-Way Intersection	Angle Same Direction (Front to Side)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	On Roadway	1	South	Changing Lanes	1 Improper Turn or Signal	1 Driver	Shoulder and Lap Belt	No Apparent Injury	Darkness	Clear	Dry	N	N	23-00024118	
05/06/2023	1712	11092	WY 212 COLLEGE DR 85 BUS US 85 BUS	COLLEGE DR 85 BUS GREELEY HWY	Intersection	Four (4)-Way Intersection	Angle Same Direction (Front to Side)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	On Roadway	1	East	Turning Right	1 No Improper Driving	1 Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	Dry	N	N	23-00047850	
05/02/2023	638	11092	US 85 BUS GREELEY HWY	COLLEGE DR	Intersection Related	Four (4)-Way Intersection	Rear End (Front to Rear)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	On Roadway	1	North	Stopped in Traffic	1 No Improper Driving	1 Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	Dry	N	N	23-00041907	
07/13/2023	910	11092	125 BUS WY 212 COLLEGE DR 85 BUS GREELEY HWY	COLLEGE DR 85 BUS WY 212	Intersection	Four (4)-Way Intersection	Angle (Front to Side), Opposing Direction	SUSPECTED MINOR INJURY	0	1	Motor Vehicle in Transport on Roadway	On Roadway	1	South	Straight Ahead	1 No Improper Driving	1 Driver	Shoulder and Lap Belt	Suspected Minor Injury	Daylight	Clear	Dry	N	N	23-00068004	

DATE	TIME	ID#	CRASH STREET	CROSS STREET	JUNCTION RELATION	INTERSECTION TYPE	NUMBER OF COLLISION	CRASH SEVERITY	FATAL INJURY	FIRST HARMFUL EVENT (FHE)	VEHICLE #	DIRECTION	MANEUVER	DRIVER ACTION	PERSON TYPE	SAFETY EQUIPMENT USED	INJURY STATUS	LIGHTING	WEATHER CONDITION	ROAD CONDITION	ALCOHOL INVOLVED	DRUGS INVOLVED	CRASH CASE#	
09/28/2023	1100	11092	126 BUSJUS BUSJUS BUSJUS GREELEY HWY	GREELEY HWY	Intersection	Four (4)Way Intersection	Angle Right Front to Side, includes Broadside	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	2	East	Straight Ahead	2 No Improper Driving	1 Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	Dry	N	N	41.102340 -104.800530 23-00104314
10/28/2023	825	11092	WV 212 COLLEGE DR		Intersection Related	Four (4)Way Intersection	Angle (Front to Side) Opposing Direction	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	2	West	Turning Right	1 Improper Turn or No Signal	1 Driver	Shoulder and Lap Belt	No Apparent Injury	Overcast	Ice/Frost	N	N	41.102343 -104.800546 23-00104246	
10/28/2023	1987	11092	WV 212 COLLEGE DR		Intersection Related	Four (4)Way Intersection	Head On (Front to Front)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	2	West	Slowing	1 No Improper Driving	1 Driver	Shoulder and Lap Belt	No Apparent Injury	Overcast	Ice/Frost	N	N	41.102318 -104.800535 23-00104433	
12/28/2023	1610	11092	WV 212 COLLEGE DR		Intersection	Four (4)Way Intersection	Angle (Front to Side) Opposing Direction	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	2	East	Straight Ahead	2 No Improper Driving	1 Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	Dry	N	N	41.102340 -104.800446 23-00122034
2024																								
01/27/2024	2150	11092	WV 212 COLLEGE DR	GREELEY HWY	Intersection	Four (4)Way Intersection	Angle (Front to Side) Opposing Direction	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	2	West	Straight Ahead	1 No Improper Driving	1 Driver	Shoulder and Lap Belt	No Apparent Injury	Overcast	Clear	Dry	N	N	41.102321 -104.800516 24-00006873
02/27/2024	600	11092	WV 212 COLLEGE DR		Intersection Related	Four (4)Way Intersection	Angle (Front to Side) Opposing Direction	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	2	East	Turning Left	2 Failed to Yield ROW	2 Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Overcast	Ice/Frost	N	N	41.102321 -104.800214 24-00016433
02/27/2024	856	11092	WV 212 COLLEGE DR		Intersection Related	Four (4)Way Intersection	Angle (Front to Side) Opposing Direction	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	2	West	Turning Right	2 Drove too Fast for Conditions	2 Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Overcast	Ice/Frost	N	N	41.102377 -104.800392 24-00016457
02/27/2024	735	11092	WV 212 COLLEGE DR	GREELEY HWY	Intersection	Four (4)Way Intersection	Angle (Front to Side) Opposing Direction	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	2	North	Turning Right	2 No Improper Driving	2 Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Overcast	Ice/Frost	N	N	41.102324 -104.800446 24-00016462
02/27/2024	1614	11092	WV 212 COLLEGE DR	GREELEY HWY	Intersection	Four (4)Way Intersection	Angle (Front to Side) Opposing Direction	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	2	East	Straight Ahead	2 Drove too Fast for Conditions	2 Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Overcast	Ice/Frost	N	N	41.102325 -104.800538 24-00016483
05/22/2024	1614	11092	WV 212 COLLEGE DR	GREELEY HWY	Intersection	Four (4)Way Intersection	Angle (Front to Side) Opposing Direction	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	2	West	Turning Right	1 Failed to Keep Proper Lane	1 Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	Ice/Frost	N	N	41.102325 -104.800443 24-00016483
06/05/2024	1248	11092	126 BUSJUS BUSJUS BUSJUS GREELEY HWY		Intersection Related	Four (4)Way Intersection	Not a Collision w/2 Vehicles in Transport	SUSPECTED MINOR INJURY	0	1	Pedestrian	1	West	Turning Right	1 Failed to Keep Proper Lane	1 Pedestrian	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	Dry	N	N	41.102485 -104.800443 24-00016483
07/23/2024	1550	11092	126 BUSJUS BUSJUS BUSJUS GREELEY HWY		Non-Junction	Not an Intersection	Sidewalk Same Direction (Passing)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	2	South	Changing Lanes	2 Improper Turn or No Signal	2 Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Clear	Dry	N	N	41.102542 -104.800446 24-00068928

DATE	TIME	ID #	CROSS STREET	CROSS STREET	JUNCTION RELATION	INTERSECTION TYPE	MANNER OF COLLISION	CRASH SEVERITY	FATAL INJURY	FIRST HARMFUL EVENT (PRE)	PHE LOCATION	#	DIRECTION	VEHICLE MANEUVER	DRIVER ACTION	PERSON TYPE	SAFETY EQUIPMENT USED	INJURY STATUS	LIGHTING	WEATHER CONDITION	ROAD CONDITION	ALCOHOL INVOLVED	DRUGS INVOLVED	CRASH CASE #
08/24/2024	1238	11092	US 85/US 85/US GREELEY HWY	125 BUS/US 85/US 85 BUS/US GREELEY HWY	Intersection	Four (4)-Way Intersection	Angle (Front to Side), Opposing Direction	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	On Roadway	1	South	Turning Left	1 Failed to Yield ROW	Driver	Shoulder and Lap Belt	No Apparent Injury	Daylight	Dry	N	N	24-00020992
10/16/2024	2330	11092	WY 212/COLLEGE DR	85 BUS/US 85/US 85 BUS/US GREELEY HWY	Non-Junction Intersection	Not an Intersection	Not a Collision w/2 Vehicles in Transport	PROPERTY DAMAGE ONLY	0	0	Other Non-Fixed Object	On Roadway	1	West	Straight Ahead	1 No Improper Driving	Driver	Shoulder and Lap Belt	No Apparent Injury	Darkness Unlighted	Dry	N	N	24-00102626
10/30/2024	1820	11092	US 85/US 85/US GREELEY HWY	US 85/US 85/US 85 BUS/US GREELEY HWY	Intersection	Four (4)-Way Intersection	Rear End (Front to Rear)	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	On Roadway	1	North	Stopped in Traffic	1 No Improper Driving	Driver	Shoulder and Lap Belt	No Apparent Injury	Darkness Unlighted	Wet	N	N	24-00106900
11/01/2024	1920	11092	US 85/US 85/US GREELEY HWY	125 BUS/US 85/US 85 BUS/US GREELEY HWY	Intersection	Four (4)-Way Intersection	Angle Right (Front to Side), Opposing Direction	PROPERTY DAMAGE ONLY	0	0	Motor Vehicle in Transport on Roadway	On Roadway	1	West	Straight Ahead	1 No Improper Driving	Driver	Shoulder and Lap Belt	No Apparent Injury	Darkness Unlighted	Dry	N	N	24-00107163

REPORT SUMMARY	
TOTAL CRASHES:	46
FATAL CRASHES:	1
INJURY CRASHES:	7
PDO CRASHES:	38
TOTAL FATALITIES:	1
TOTAL INJURIES:	8

INJURY COUNTS

YEAR	FATAL CRASHES		CRITICAL CRASHES		SERIOUS CRASHES		TOTAL CRASHES		FATAL INJURIES		SERIOUS INJURIES		MINOR INJURIES		POSSIBLE INJURIES	
	0	1	0	1	0	2	7	9	0	0	0	0	1	2	0	24
2020	0	2	0	0	0	2	7	9	0	0	0	0	1	2	0	24
2021	0	2	0	0	0	2	5	7	0	0	0	0	2	0	0	10
2022	0	1	0	0	0	1	8	9	0	0	0	0	1	8	1	17
2023	0	1	0	0	0	1	8	9	0	0	0	0	1	0	0	25
2024	1	1	1	1	1	1	10	12	1	1	0	2	0	0	24	
	1	7	1	1	1	7	38	46	1	0	6	2	0	2	109	

- * PDO = Property Damage Only Crashes
- * Critical Crashes = Fatal & Serious Injury Crashes
- * Serious Crashes = Suspected Minor Injury & Possible Injury Crashes
- * Damage Crashes = No Apparent Injury & Unknown Injury Crashes

**EXISTING (2025) CONDITIONS
LEVEL OF SERVICE
CALCULATIONS**

Intersection						
Int Delay, s/veh	5.9					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	2	3	39	34	4	59
Future Vol, veh/h	2	3	39	34	4	59
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	68	68	68	68	68	68
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	3	4	57	50	6	87

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	7	0	170
Stage 1	-	-	-	-	5
Stage 2	-	-	-	-	165
Critical Hdwy	-	-	4.12	-	6.42
Critical Hdwy Stg 1	-	-	-	-	5.42
Critical Hdwy Stg 2	-	-	-	-	5.42
Follow-up Hdwy	-	-	2.218	-	3.518
Pot Cap-1 Maneuver	-	-	1613	-	820
Stage 1	-	-	-	-	1018
Stage 2	-	-	-	-	865
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1613	-	790
Mov Cap-2 Maneuver	-	-	-	-	790
Stage 1	-	-	-	-	1018
Stage 2	-	-	-	-	833

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	0	3.91	8.75
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	1054	-	-	962	-
HCM Lane V/C Ratio	0.088	-	-	0.036	-
HCM Ctrl Dly (s/v)	8.7	-	-	7.3	0
HCM Lane LOS	A	-	-	A	A
HCM 95th %tile Q(veh)	0.3	-	-	0.1	-

Intersection												
Int Delay, s/veh	2.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↕		↕	↕	↕
Traffic Vol, veh/h	103	3	5	0	1	4	10	436	1	1	286	111
Future Vol, veh/h	103	3	5	0	1	4	10	436	1	1	286	111
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	500	-	-	500	-	125
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	112	3	5	0	1	4	11	474	1	1	311	121

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	572	810	155	655	930	238	432	0	0	475	0	0
Stage 1	313	313	-	496	496	-	-	-	-	-	-	-
Stage 2	259	497	-	159	434	-	-	-	-	-	-	-
Critical Hdwy	7.54	6.54	6.94	7.54	6.54	6.94	4.14	-	-	4.14	-	-
Critical Hdwy Stg 1	6.54	5.54	-	6.54	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.54	5.54	-	6.54	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.52	4.02	3.32	3.52	4.02	3.32	2.22	-	-	2.22	-	-
Pot Cap-1 Maneuver	403	312	863	351	266	764	1124	-	-	1083	-	-
Stage 1	672	655	-	524	544	-	-	-	-	-	-	-
Stage 2	723	543	-	827	580	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	395	309	863	341	263	764	1124	-	-	1083	-	-
Mov Cap-2 Maneuver	395	309	-	341	263	-	-	-	-	-	-	-
Stage 1	672	655	-	519	538	-	-	-	-	-	-	-
Stage 2	710	538	-	817	579	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Ctrl Dly, s/v	17.78		11.57		0.18		0.02	
HCM LOS	C		B					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1124	-	-	401	553	1083	-	-
HCM Lane V/C Ratio	0.01	-	-	0.301	0.01	0.001	-	-
HCM Ctrl Dly (s/v)	8.2	-	-	17.8	11.6	8.3	-	-
HCM Lane LOS	A	-	-	C	B	A	-	-
HCM 95th %tile Q(veh)	0	-	-	1.2	0	0	-	-

HCM 7th Signalized Intersection Summary

3: S. Greeley Hwy & College Dr

12/17/2025



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗	↗	↖	↗	↗	↖	↕	↕	↖	↗	↖
Traffic Volume (veh/h)	177	230	103	146	174	90	109	513	183	68	315	136
Future Volume (veh/h)	177	230	103	146	174	90	109	513	183	68	315	136
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	188	245	110	155	185	96	116	546	195	72	335	145
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	330	290	245	281	258	219	522	1217	433	394	1115	473
Arrive On Green	0.11	0.15	0.15	0.09	0.14	0.14	0.05	0.47	0.47	0.04	0.46	0.46
Sat Flow, veh/h	1781	1870	1585	1781	1870	1585	1781	2569	914	1781	2429	1032
Grp Volume(v), veh/h	188	245	110	155	185	96	116	377	364	72	243	237
Grp Sat Flow(s),veh/h/ln	1781	1870	1585	1781	1870	1585	1781	1777	1706	1781	1777	1685
Q Serve(g_s), s	7.3	10.6	5.2	5.9	7.8	4.6	2.8	11.7	11.8	1.7	7.1	7.3
Cycle Q Clear(g_c), s	7.3	10.6	5.2	5.9	7.8	4.6	2.8	11.7	11.8	1.7	7.1	7.3
Prop In Lane	1.00		1.00	1.00		1.00	1.00		0.54	1.00		0.61
Lane Grp Cap(c), veh/h	330	290	245	281	258	219	522	842	808	394	815	773
V/C Ratio(X)	0.57	0.85	0.45	0.55	0.72	0.44	0.22	0.45	0.45	0.18	0.30	0.31
Avail Cap(c_a), veh/h	330	386	327	311	386	327	555	842	808	475	815	773
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	26.6	34.0	31.8	26.4	34.1	32.7	10.9	14.6	14.6	11.2	14.1	14.1
Incr Delay (d2), s/veh	1.5	9.8	0.5	0.6	1.4	0.5	0.1	1.7	1.8	0.1	0.9	1.0
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	3.2	5.5	1.9	2.4	3.5	1.7	0.9	4.4	4.2	0.6	2.7	2.6
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	28.1	43.8	32.3	27.1	35.5	33.3	11.0	16.3	16.4	11.3	15.0	15.1
LnGrp LOS	C	D	C	C	D	C	B	B	B	B	B	B
Approach Vol, veh/h		543			436			857			552	
Approach Delay, s/veh		36.0			32.0			15.6			14.6	
Approach LOS		D			C			B			B	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	8.2	44.2	12.6	17.7	9.5	43.0	14.0	16.3				
Change Period (Y+Rc), s	5.0	5.0	4.9	4.9	5.0	5.0	4.9	4.9				
Max Green Setting (Gmax), s	7.0	37.0	9.1	17.1	6.0	38.0	9.1	17.1				
Max Q Clear Time (g_c+I1), s	3.7	13.8	7.9	12.6	4.8	9.3	9.3	9.8				
Green Ext Time (p_c), s	0.0	1.2	0.0	0.3	0.0	0.8	0.0	0.2				

Intersection Summary												
HCM 7th Control Delay, s/veh											23.0	
HCM 7th LOS											C	

Notes
User approved pedestrian interval to be less than phase max green.

Intersection						
Int Delay, s/veh	6					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	29	1	67	10	9	32
Future Vol, veh/h	29	1	67	10	9	32
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	61	61	61	61	61	61
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	48	2	110	16	15	52

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	49	0	284
Stage 1	-	-	-	-	48
Stage 2	-	-	-	-	236
Critical Hdwy	-	-	4.12	-	6.42
Critical Hdwy Stg 1	-	-	-	-	5.42
Critical Hdwy Stg 2	-	-	-	-	5.42
Follow-up Hdwy	-	-	2.218	-	3.518
Pot Cap-1 Maneuver	-	-	1558	-	706
Stage 1	-	-	-	-	974
Stage 2	-	-	-	-	803
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1558	-	656
Mov Cap-2 Maneuver	-	-	-	-	656
Stage 1	-	-	-	-	974
Stage 2	-	-	-	-	746

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	0	6.51	9.27
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	909	-	-	1543	-
HCM Lane V/C Ratio	0.074	-	-	0.071	-
HCM Ctrl Dly (s/v)	9.3	-	-	7.5	0
HCM Lane LOS	A	-	-	A	A
HCM 95th %tile Q(veh)	0.2	-	-	0.2	-

Intersection												
Int Delay, s/veh	3.8											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↕		↕	↕	↕
Traffic Vol, veh/h	110	3	8	0	8	0	4	535	1	6	509	72
Future Vol, veh/h	110	3	8	0	8	0	4	535	1	6	509	72
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	500	-	-	500	-	125
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	120	3	9	0	9	0	4	582	1	7	553	78

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	870	1158	277	882	1235	291	632	0	0	583	0	0
Stage 1	566	566	-	591	591	-	-	-	-	-	-	-
Stage 2	304	591	-	291	645	-	-	-	-	-	-	-
Critical Hdwy	7.54	6.54	6.94	7.54	6.54	6.94	4.14	-	-	4.14	-	-
Critical Hdwy Stg 1	6.54	5.54	-	6.54	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.54	5.54	-	6.54	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.52	4.02	3.32	3.52	4.02	3.32	2.22	-	-	2.22	-	-
Pot Cap-1 Maneuver	245	195	721	241	175	705	947	-	-	988	-	-
Stage 1	476	505	-	460	493	-	-	-	-	-	-	-
Stage 2	681	493	-	692	466	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	231	193	721	231	173	705	947	-	-	988	-	-
Mov Cap-2 Maneuver	231	193	-	231	173	-	-	-	-	-	-	-
Stage 1	473	502	-	458	491	-	-	-	-	-	-	-
Stage 2	666	490	-	675	463	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Ctrl Dly, s/v	36.72		26.9		0.07		0.09	
HCM LOS	E		D					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	947	-	-	240	173	988	-	-
HCM Lane V/C Ratio	0.005	-	-	0.547	0.05	0.007	-	-
HCM Ctrl Dly (s/v)	8.8	-	-	36.7	26.9	8.7	-	-
HCM Lane LOS	A	-	-	E	D	A	-	-
HCM 95th %tile Q(veh)	0	-	-	3	0.2	0	-	-

HCM 7th Signalized Intersection Summary
 3: S. Greeley Hwy & College Dr

12/17/2025



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↙	↑	↗	↙	↑	↗	↙	↑↑		↙	↑↑	
Traffic Volume (veh/h)	138	181	109	204	186	108	94	458	212	98	508	146
Future Volume (veh/h)	138	181	109	204	186	108	94	458	212	98	508	146
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	144	189	114	213	194	113	98	477	221	102	529	152
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	330	243	206	359	325	276	403	997	459	396	1155	330
Arrive On Green	0.08	0.13	0.13	0.13	0.17	0.17	0.05	0.42	0.42	0.05	0.42	0.42
Sat Flow, veh/h	1781	1870	1585	1781	1870	1585	1781	2363	1088	1781	2727	780
Grp Volume(v), veh/h	144	189	114	213	194	113	98	358	340	102	344	337
Grp Sat Flow(s),veh/h/ln	1781	1870	1585	1781	1870	1585	1781	1777	1675	1781	1777	1730
Q Serve(g_s), s	4.8	7.2	5.0	7.4	7.0	4.7	2.3	10.7	10.8	2.3	10.2	10.3
Cycle Q Clear(g_c), s	4.8	7.2	5.0	7.4	7.0	4.7	2.3	10.7	10.8	2.3	10.2	10.3
Prop In Lane	1.00		1.00	1.00		1.00	1.00		0.65	1.00		0.45
Lane Grp Cap(c), veh/h	330	243	206	359	325	276	403	749	706	396	753	733
V/C Ratio(X)	0.44	0.78	0.55	0.59	0.60	0.41	0.24	0.48	0.48	0.26	0.46	0.46
Avail Cap(c_a), veh/h	330	537	455	498	766	649	459	749	706	449	753	733
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	22.5	31.0	30.0	23.5	28.0	27.0	11.6	15.4	15.4	11.7	15.1	15.2
Incr Delay (d2), s/veh	0.3	2.0	0.9	0.6	0.7	0.4	0.1	2.2	2.3	0.1	2.0	2.1
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.9	3.3	1.8	2.9	3.0	1.7	0.7	4.0	3.9	0.8	3.8	3.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	22.8	33.0	30.8	24.1	28.6	27.4	11.7	17.6	17.8	11.8	17.1	17.2
LnGrp LOS	C	C	C	C	C	C	B	B	B	B	B	B
Approach Vol, veh/h		447			520			796			783	
Approach Delay, s/veh		29.2			26.5			16.9			16.5	
Approach LOS		C			C			B			B	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	8.8	36.0	14.2	14.4	8.7	36.1	11.0	17.7				
Change Period (Y+Rc), s	5.0	5.0	4.9	4.9	5.0	5.0	4.9	4.9				
Max Green Setting (Gmax), s	6.0	31.0	15.1	21.1	6.0	31.0	6.1	30.1				
Max Q Clear Time (g_c+I1), s	4.3	12.8	9.4	9.2	4.3	12.3	6.8	9.0				
Green Ext Time (p_c), s	0.0	1.1	0.0	0.4	0.0	1.1	0.0	0.4				
Intersection Summary												
HCM 7th Control Delay, s/veh			20.9									
HCM 7th LOS			C									

**SHORT-TERM (2029) CONDITIONS
WITHOUT PROJECT
LEVEL OF SERVICE
CALCULATIONS**

Intersection						
Int Delay, s/veh	5.9					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	2	3	40	35	4	60
Future Vol, veh/h	2	3	40	35	4	60
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	68	68	68	68	68	68
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	3	4	59	51	6	88

Major/Minor	Major1	Major2	Minor1		
Conflicting Flow All	0	0	7	0	174
Stage 1	-	-	-	-	5
Stage 2	-	-	-	-	169
Critical Hdwy	-	-	4.12	-	6.42
Critical Hdwy Stg 1	-	-	-	-	5.42
Critical Hdwy Stg 2	-	-	-	-	5.42
Follow-up Hdwy	-	-	2.218	-	3.518
Pot Cap-1 Maneuver	-	-	1613	-	816
Stage 1	-	-	-	-	1018
Stage 2	-	-	-	-	861
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1613	-	785
Mov Cap-2 Maneuver	-	-	-	-	785
Stage 1	-	-	-	-	1018
Stage 2	-	-	-	-	828

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	0	3.9	8.75
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	1053	-	-	960	-
HCM Lane V/C Ratio	0.089	-	-	0.036	-
HCM Ctrl Dly (s/v)	8.8	-	-	7.3	0
HCM Lane LOS	A	-	-	A	A
HCM 95th %tile Q(veh)	0.3	-	-	0.1	-

Intersection												
Int Delay, s/veh	2.3											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↕		↕	↕	↕
Traffic Vol, veh/h	105	3	5	0	1	4	10	445	1	1	292	113
Future Vol, veh/h	105	3	5	0	1	4	10	445	1	1	292	113
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	500	-	-	500	-	125
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	114	3	5	0	1	4	11	484	1	1	317	123

Major/Minor	Minor2		Minor1			Major1		Major2				
Conflicting Flow All	584	826	159	668	948	242	440	0	0	485	0	0
Stage 1	320	320	-	506	506	-	-	-	-	-	-	-
Stage 2	264	507	-	162	442	-	-	-	-	-	-	-
Critical Hdwy	7.54	6.54	6.94	7.54	6.54	6.94	4.14	-	-	4.14	-	-
Critical Hdwy Stg 1	6.54	5.54	-	6.54	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.54	5.54	-	6.54	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.52	4.02	3.32	3.52	4.02	3.32	2.22	-	-	2.22	-	-
Pot Cap-1 Maneuver	395	306	858	344	259	758	1116	-	-	1074	-	-
Stage 1	666	651	-	517	538	-	-	-	-	-	-	-
Stage 2	718	538	-	823	574	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	387	302	858	334	256	758	1116	-	-	1074	-	-
Mov Cap-2 Maneuver	387	302	-	334	256	-	-	-	-	-	-	-
Stage 1	666	651	-	512	533	-	-	-	-	-	-	-
Stage 2	706	533	-	813	574	-	-	-	-	-	-	-

Approach	EB		WB			NB		SB		
HCM Ctrl Dly, s/v	18.23		11.67			0.18		0.02		
HCM LOS	C		B							

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1116	-	-	394	545	1074	-	-
HCM Lane V/C Ratio	0.01	-	-	0.312	0.01	0.001	-	-
HCM Ctrl Dly (s/v)	8.3	-	-	18.2	11.7	8.4	-	-
HCM Lane LOS	A	-	-	C	B	A	-	-
HCM 95th %tile Q(veh)	0	-	-	1.3	0	0	-	-

HCM 7th Signalized Intersection Summary
 3: S. Greeley Hwy & College Dr

12/19/2025



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↙	↑	↗	↙	↑	↗	↙	↑↕		↙	↑↕	
Traffic Volume (veh/h)	192	249	111	158	188	97	118	523	198	74	321	147
Future Volume (veh/h)	192	249	111	158	188	97	118	523	198	74	321	147
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	204	265	118	168	200	103	126	556	211	79	341	156
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	334	307	260	286	291	247	505	1172	444	375	1064	478
Arrive On Green	0.11	0.16	0.16	0.10	0.16	0.16	0.06	0.46	0.46	0.04	0.45	0.45
Sat Flow, veh/h	1781	1870	1585	1781	1870	1585	1781	2521	954	1781	2384	1071
Grp Volume(v), veh/h	204	265	118	168	200	103	126	391	376	79	253	244
Grp Sat Flow(s),veh/h/ln	1781	1870	1585	1781	1870	1585	1781	1777	1699	1781	1777	1678
Q Serve(g_s), s	8.1	11.7	5.7	6.5	8.6	5.0	3.2	12.9	12.9	2.0	7.8	8.0
Cycle Q Clear(g_c), s	8.1	11.7	5.7	6.5	8.6	5.0	3.2	12.9	12.9	2.0	7.8	8.0
Prop In Lane	1.00		1.00	1.00		1.00	1.00		0.56	1.00		0.64
Lane Grp Cap(c), veh/h	334	307	260	286	291	247	505	826	790	375	793	749
V/C Ratio(X)	0.61	0.86	0.45	0.59	0.69	0.42	0.25	0.47	0.48	0.21	0.32	0.33
Avail Cap(c_a), veh/h	334	376	318	301	376	318	527	826	790	450	793	749
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	26.7	34.6	32.1	26.5	34.0	32.4	11.7	15.6	15.6	12.0	15.2	15.3
Incr Delay (d2), s/veh	2.4	13.7	0.5	1.6	1.9	0.4	0.1	1.9	2.1	0.1	1.1	1.2
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	3.6	6.4	2.1	2.7	3.9	1.9	1.1	4.9	4.7	0.7	3.0	2.9
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	29.0	48.4	32.6	28.2	35.8	32.8	11.8	17.6	17.7	12.1	16.3	16.4
LnGrp LOS	C	D	C	C	D	C	B	B	B	B	B	B
Approach Vol, veh/h		587			471			893			576	
Approach Delay, s/veh		38.5			32.4			16.8			15.8	
Approach LOS		D			C			B			B	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	8.4	44.6	13.3	18.9	9.9	43.0	14.0	18.2				
Change Period (Y+Rc), s	5.0	5.0	4.9	4.9	5.0	5.0	4.9	4.9				
Max Green Setting (Gmax), s	7.0	37.0	9.1	17.1	6.0	38.0	9.1	17.1				
Max Q Clear Time (g_c+I1), s	4.0	14.9	8.5	13.7	5.2	10.0	10.1	10.6				
Green Ext Time (p_c), s	0.0	1.3	0.0	0.2	0.0	0.8	0.0	0.2				

Intersection Summary												
HCM 7th Control Delay, s/veh			24.5									
HCM 7th LOS			C									

Notes
 User approved pedestrian interval to be less than phase max green.

Intersection						
Int Delay, s/veh	6					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	30	1	68	10	9	33
Future Vol, veh/h	30	1	68	10	9	33
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	61	61	61	61	61	61
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	49	2	111	16	15	54

Major/Minor	Major1	Major2	Minor1	Minor2
Conflicting Flow All	0	0	51	289
Stage 1	-	-	-	50
Stage 2	-	-	-	239
Critical Hdwy	-	-	4.12	6.42
Critical Hdwy Stg 1	-	-	-	5.42
Critical Hdwy Stg 2	-	-	-	5.42
Follow-up Hdwy	-	-	2.218	3.518
Pot Cap-1 Maneuver	-	-	1555	701
Stage 1	-	-	-	972
Stage 2	-	-	-	800
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	-	-	1555	651
Mov Cap-2 Maneuver	-	-	-	651
Stage 1	-	-	-	972
Stage 2	-	-	-	743

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	0	6.53	9.29
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	908	-	-	1541	-
HCM Lane V/C Ratio	0.076	-	-	0.072	-
HCM Ctrl Dly (s/v)	9.3	-	-	7.5	0
HCM Lane LOS	A	-	-	A	A
HCM 95th %tile Q(veh)	0.2	-	-	0.2	-

Intersection												
Int Delay, s/veh	4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↕		↕	↕	↕
Traffic Vol, veh/h	112	3	8	0	8	0	4	546	1	6	519	73
Future Vol, veh/h	112	3	8	0	8	0	4	546	1	6	519	73
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	500	-	-	500	-	125
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	122	3	9	0	9	0	4	593	1	7	564	79

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	887	1180	282	899	1259	297	643	0	0	595	0	0
Stage 1	577	577	-	603	603	-	-	-	-	-	-	-
Stage 2	310	603	-	297	657	-	-	-	-	-	-	-
Critical Hdwy	7.54	6.54	6.94	7.54	6.54	6.94	4.14	-	-	4.14	-	-
Critical Hdwy Stg 1	6.54	5.54	-	6.54	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.54	5.54	-	6.54	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.52	4.02	3.32	3.52	4.02	3.32	2.22	-	-	2.22	-	-
Pot Cap-1 Maneuver	239	189	715	234	169	699	937	-	-	978	-	-
Stage 1	469	500	-	453	487	-	-	-	-	-	-	-
Stage 2	675	486	-	687	460	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	224	187	715	224	167	699	937	-	-	978	-	-
Mov Cap-2 Maneuver	224	187	-	224	167	-	-	-	-	-	-	-
Stage 1	466	496	-	451	484	-	-	-	-	-	-	-
Stage 2	660	484	-	670	457	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Ctrl Dly, s/v	39.35		27.68		0.06		0.09	
HCM LOS	E		D					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	937	-	-	233	167	978	-	-
HCM Lane V/C Ratio	0.005	-	-	0.573	0.052	0.007	-	-
HCM Ctrl Dly (s/v)	8.9	-	-	39.3	27.7	8.7	-	-
HCM Lane LOS	A	-	-	E	D	A	-	-
HCM 95th %tile Q(veh)	0	-	-	3.2	0.2	0	-	-

HCM 7th Signalized Intersection Summary
 3: S. Greeley Hwy & College Dr

12/19/2025



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↙	↑	↗	↙	↑	↗	↙	↑↕		↙	↑↕	
Traffic Volume (veh/h)	149	196	118	221	201	117	102	467	229	106	518	158
Future Volume (veh/h)	149	196	118	221	201	117	102	467	229	106	518	158
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	155	204	123	230	209	122	106	486	239	110	540	165
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	332	256	217	367	356	302	387	950	465	379	1107	337
Arrive On Green	0.08	0.14	0.14	0.13	0.19	0.19	0.05	0.41	0.41	0.06	0.41	0.41
Sat Flow, veh/h	1781	1870	1585	1781	1870	1585	1781	2313	1131	1781	2683	817
Grp Volume(v), veh/h	155	204	123	230	209	122	106	373	352	110	357	348
Grp Sat Flow(s),veh/h/ln	1781	1870	1585	1781	1870	1585	1781	1777	1667	1781	1777	1723
Q Serve(g_s), s	5.2	8.0	5.5	8.2	7.7	5.1	2.5	11.8	11.9	2.7	11.1	11.2
Cycle Q Clear(g_c), s	5.2	8.0	5.5	8.2	7.7	5.1	2.5	11.8	11.9	2.7	11.1	11.2
Prop In Lane	1.00		1.00	1.00		1.00	1.00		0.68	1.00		0.47
Lane Grp Cap(c), veh/h	332	256	217	367	356	302	387	730	685	379	733	711
V/C Ratio(X)	0.47	0.80	0.57	0.63	0.59	0.40	0.27	0.51	0.51	0.29	0.49	0.49
Avail Cap(c_a), veh/h	332	523	443	485	746	632	433	730	685	421	733	711
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	22.4	31.5	30.5	23.5	27.8	26.8	12.4	16.6	16.6	12.6	16.3	16.3
Incr Delay (d2), s/veh	0.4	2.1	0.9	0.7	0.6	0.3	0.1	2.5	2.7	0.2	2.3	2.4
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.1	3.6	2.0	3.2	3.3	1.8	0.8	4.6	4.3	0.9	4.3	4.2
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	22.8	33.7	31.3	24.2	28.4	27.1	12.5	19.1	19.4	12.7	18.6	18.7
LnGrp LOS	C	C	C	C	C	C	B	B	B	B	B	B
Approach Vol, veh/h		482			561			831			815	
Approach Delay, s/veh		29.6			26.4			18.4			17.9	
Approach LOS		C			C			B			B	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	9.2	36.0	15.0	15.2	9.1	36.1	11.0	19.3				
Change Period (Y+Rc), s	5.0	5.0	4.9	4.9	5.0	5.0	4.9	4.9				
Max Green Setting (Gmax), s	6.0	31.0	15.1	21.1	6.0	31.0	6.1	30.1				
Max Q Clear Time (g_c+I1), s	4.7	13.9	10.2	10.0	4.5	13.2	7.2	9.7				
Green Ext Time (p_c), s	0.0	1.2	0.0	0.4	0.0	1.1	0.0	0.4				
Intersection Summary												
HCM 7th Control Delay, s/veh			21.9									
HCM 7th LOS			C									

**SHORT-TERM (2029) CONDITIONS
WITH PROJECT
LEVEL OF SERVICE
CALCULATIONS**

Intersection						
Int Delay, s/veh	2.1					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↶		↷	↶	↷	
Traffic Vol, veh/h	192	15	40	266	19	60
Future Vol, veh/h	192	15	40	266	19	60
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	100	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	256	20	53	355	25	80

Major/Minor	Major1	Major2	Minor1			
Conflicting Flow All	0	0	276	0	727	266
Stage 1	-	-	-	-	266	-
Stage 2	-	-	-	-	461	-
Critical Hdwy	-	-	4.12	-	6.42	6.22
Critical Hdwy Stg 1	-	-	-	-	5.42	-
Critical Hdwy Stg 2	-	-	-	-	5.42	-
Follow-up Hdwy	-	-	2.218	-	3.518	3.318
Pot Cap-1 Maneuver	-	-	1287	-	391	773
Stage 1	-	-	-	-	779	-
Stage 2	-	-	-	-	635	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1287	-	375	773
Mov Cap-2 Maneuver	-	-	-	-	375	-
Stage 1	-	-	-	-	779	-
Stage 2	-	-	-	-	608	-

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	0	1.04	12.05
HCM LOS			B

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	615	-	-	1287	-
HCM Lane V/C Ratio	0.171	-	-	0.041	-
HCM Ctrl Dly (s/v)	12.1	-	-	7.9	-
HCM Lane LOS	B	-	-	A	-
HCM 95th %tile Q(veh)	0.6	-	-	0.1	-

Intersection												
Int Delay, s/veh	11.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↕		↕	↕	↕
Traffic Vol, veh/h	283	3	15	0	1	4	22	445	1	1	292	329
Future Vol, veh/h	283	3	15	0	1	4	22	445	1	1	292	329
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	500	-	-	500	-	125
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	308	3	16	0	1	4	24	484	1	1	317	358

Major/Minor	Minor2		Minor1			Major1			Major2			
Conflicting Flow All	610	852	159	695	1209	242	675	0	0	485	0	0
Stage 1	320	320	-	532	532	-	-	-	-	-	-	-
Stage 2	290	533	-	162	677	-	-	-	-	-	-	-
Critical Hdwy	7.54	6.54	6.94	7.54	6.54	6.94	4.14	-	-	4.14	-	-
Critical Hdwy Stg 1	6.54	5.54	-	6.54	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.54	5.54	-	6.54	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.52	4.02	3.32	3.52	4.02	3.32	2.22	-	-	2.22	-	-
Pot Cap-1 Maneuver	379	295	858	329	181	758	912	-	-	1074	-	-
Stage 1	666	651	-	499	524	-	-	-	-	-	-	-
Stage 2	693	523	-	823	450	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	364	287	858	310	177	758	912	-	-	1074	-	-
Mov Cap-2 Maneuver	364	287	-	310	177	-	-	-	-	-	-	-
Stage 1	666	651	-	486	510	-	-	-	-	-	-	-
Stage 2	670	510	-	803	450	-	-	-	-	-	-	-

Approach	EB		WB			NB			SB		
HCM Ctrl Dly, s/v	54.29		12.97			0.43			0.01		
HCM LOS	F		B								

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	912	-	-	374	457	1074	-	-
HCM Lane V/C Ratio	0.026	-	-	0.875	0.012	0.001	-	-
HCM Ctrl Dly (s/v)	9.1	-	-	54.3	13	8.4	-	-
HCM Lane LOS	A	-	-	F	B	A	-	-
HCM 95th %tile Q(veh)	0.1	-	-	8.5	0	0	-	-

HCM 7th Signalized Intersection Summary
 3: S. Greeley Hwy & College Dr

12/19/2025



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗	↗	↖	↗	↗	↖	↕	↕	↖	↗	↖
Traffic Volume (veh/h)	192	249	136	256	188	97	138	584	279	74	395	147
Future Volume (veh/h)	192	249	136	256	188	97	138	584	279	74	395	147
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	204	265	145	272	200	103	147	621	297	79	420	156
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	338	306	260	295	306	260	472	1082	517	317	1113	409
Arrive On Green	0.10	0.16	0.16	0.10	0.16	0.16	0.07	0.46	0.46	0.04	0.44	0.44
Sat Flow, veh/h	1781	1870	1585	1781	1870	1585	1781	2332	1115	1781	2544	935
Grp Volume(v), veh/h	204	265	145	272	200	103	147	473	445	79	292	284
Grp Sat Flow(s),veh/h/ln	1781	1870	1585	1781	1870	1585	1781	1777	1670	1781	1777	1702
Q Serve(g_s), s	8.2	12.0	7.3	9.1	8.7	5.0	3.9	16.9	16.9	2.0	9.6	9.8
Cycle Q Clear(g_c), s	8.2	12.0	7.3	9.1	8.7	5.0	3.9	16.9	16.9	2.0	9.6	9.8
Prop In Lane	1.00		1.00	1.00		1.00	1.00		0.67	1.00		0.55
Lane Grp Cap(c), veh/h	338	306	260	295	306	260	472	825	775	317	778	745
V/C Ratio(X)	0.60	0.86	0.56	0.92	0.65	0.40	0.31	0.57	0.57	0.25	0.38	0.38
Avail Cap(c_a), veh/h	338	368	312	295	368	312	478	825	775	390	778	745
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	26.8	35.4	33.4	30.4	34.0	32.5	12.3	17.0	17.0	13.1	16.4	16.5
Incr Delay (d2), s/veh	2.2	14.6	0.7	32.4	1.7	0.4	0.1	2.9	3.1	0.2	1.4	1.5
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	3.6	6.6	0.1	3.8	3.9	1.9	1.3	6.6	6.2	0.7	3.7	3.6
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	28.9	50.0	34.1	62.8	35.7	32.8	12.5	19.9	20.1	13.2	17.8	18.0
LnGrp LOS	C	D	C	E	D	C	B	B	C	B	B	B
Approach Vol, veh/h		614			575			1065			655	
Approach Delay, s/veh		39.2			48.0			18.9			17.3	
Approach LOS		D			D			B			B	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	8.4	45.3	14.0	19.1	10.7	43.0	14.0	19.1				
Change Period (Y+Rc), s	5.0	5.0	4.9	4.9	5.0	5.0	4.9	4.9				
Max Green Setting (Gmax), s	7.0	37.0	9.1	17.1	6.0	38.0	9.1	17.1				
Max Q Clear Time (g_c+I1), s	4.0	18.9	11.1	14.0	5.9	11.8	10.2	10.7				
Green Ext Time (p_c), s	0.0	1.6	0.0	0.2	0.0	0.9	0.0	0.2				

Intersection Summary												
HCM 7th Control Delay, s/veh											28.6	
HCM 7th LOS											C	

Notes
 User approved pedestrian interval to be less than phase max green.

Intersection						
Int Delay, s/veh	3					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↶		↷	↶	↷	
Traffic Vol, veh/h	129	7	68	95	14	33
Future Vol, veh/h	129	7	68	95	14	33
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	100	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	75	75	75	75	75	75
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	172	9	91	127	19	44

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	181	0	485
Stage 1	-	-	-	-	177
Stage 2	-	-	-	-	308
Critical Hdwy	-	-	4.12	-	6.42
Critical Hdwy Stg 1	-	-	-	-	5.42
Critical Hdwy Stg 2	-	-	-	-	5.42
Follow-up Hdwy	-	-	2.218	-	3.518
Pot Cap-1 Maneuver	-	-	1394	-	541
Stage 1	-	-	-	-	854
Stage 2	-	-	-	-	745
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1394	-	506
Mov Cap-2 Maneuver	-	-	-	-	506
Stage 1	-	-	-	-	854
Stage 2	-	-	-	-	697

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	0	3.24	10.52
HCM LOS			B

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	715	-	-	1394	-
HCM Lane V/C Ratio	0.088	-	-	0.065	-
HCM Ctrl Dly (s/v)	10.5	-	-	7.8	-
HCM Lane LOS	B	-	-	A	-
HCM 95th %tile Q(veh)	0.3	-	-	0.2	-

Intersection												
Int Delay, s/veh	19.1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕		↕	↕		↕	↕	↕
Traffic Vol, veh/h	205	3	13	0	8	0	9	546	1	6	519	152
Future Vol, veh/h	205	3	13	0	8	0	9	546	1	6	519	152
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	500	-	-	500	-	125
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	223	3	14	0	9	0	10	593	1	7	564	165

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	898	1191	282	910	1356	297	729	0	0	595	0	0
Stage 1	577	577	-	614	614	-	-	-	-	-	-	-
Stage 2	321	614	-	297	742	-	-	-	-	-	-	-
Critical Hdwy	7.54	6.54	6.94	7.54	6.54	6.94	4.14	-	-	4.14	-	-
Critical Hdwy Stg 1	6.54	5.54	-	6.54	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.54	5.54	-	6.54	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.52	4.02	3.32	3.52	4.02	3.32	2.22	-	-	2.22	-	-
Pot Cap-1 Maneuver	234	186	715	229	148	699	870	-	-	978	-	-
Stage 1	469	500	-	446	481	-	-	-	-	-	-	-
Stage 2	665	481	-	687	420	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	~ 217	183	715	217	145	699	870	-	-	978	-	-
Mov Cap-2 Maneuver	~ 217	183	-	217	145	-	-	-	-	-	-	-
Stage 1	466	496	-	441	476	-	-	-	-	-	-	-
Stage 2	646	476	-	665	418	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Ctrl Dly, s/v	124.69		31.33		0.15		0.08	
HCM LOS	F		D					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	870	-	-	225	145	978	-	-
HCM Lane V/C Ratio	0.011	-	-	1.066	0.06	0.007	-	-
HCM Ctrl Dly (s/v)	9.2	-	-	124.7	31.3	8.7	-	-
HCM Lane LOS	A	-	-	F	D	A	-	-
HCM 95th %tile Q(veh)	0	-	-	10.5	0.2	0	-	-

Notes	
~: Volume exceeds capacity	\$: Delay exceeds 300s
+: Computation Not Defined	*: All major volume in platoon

HCM 7th Signalized Intersection Summary
 3: S. Greeley Hwy & College Dr

12/19/2025



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↙	↑	↗	↙	↑	↗	↙	↑↕		↙	↑↕	
Traffic Volume (veh/h)	149	196	127	257	201	117	113	499	271	106	545	158
Future Volume (veh/h)	149	196	127	257	201	117	113	499	271	106	545	158
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	155	204	132	268	209	122	118	520	282	110	568	165
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	348	255	216	395	392	333	371	894	484	340	1083	314
Arrive On Green	0.08	0.14	0.14	0.15	0.21	0.21	0.06	0.40	0.40	0.06	0.40	0.40
Sat Flow, veh/h	1781	1870	1585	1781	1870	1585	1781	2226	1204	1781	2718	787
Grp Volume(v), veh/h	155	204	132	268	209	122	118	415	387	110	371	362
Grp Sat Flow(s),veh/h/ln	1781	1870	1585	1781	1870	1585	1781	1777	1654	1781	1777	1729
Q Serve(g_s), s	5.3	8.2	6.1	9.8	7.7	5.1	3.0	14.2	14.2	2.8	12.3	12.4
Cycle Q Clear(g_c), s	5.3	8.2	6.1	9.8	7.7	5.1	3.0	14.2	14.2	2.8	12.3	12.4
Prop In Lane	1.00		1.00	1.00		1.00	1.00		0.73	1.00		0.46
Lane Grp Cap(c), veh/h	348	255	216	395	392	333	371	714	664	340	708	688
V/C Ratio(X)	0.45	0.80	0.61	0.68	0.53	0.37	0.32	0.58	0.58	0.32	0.52	0.53
Avail Cap(c_a), veh/h	348	507	430	470	723	613	403	714	664	378	708	688
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	22.1	32.6	31.7	23.8	27.4	26.3	13.5	18.2	18.2	13.8	17.8	17.8
Incr Delay (d2), s/veh	0.3	2.2	1.0	2.0	0.4	0.3	0.2	3.4	3.7	0.2	2.8	2.9
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.1	3.8	2.3	4.0	3.3	1.8	1.0	5.6	5.3	0.9	4.9	4.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	22.4	34.8	32.7	25.8	27.8	26.6	13.6	21.6	21.9	14.0	20.6	20.7
LnGrp LOS	C	C	C	C	C	C	B	C	C	B	C	C
Approach Vol, veh/h		491			599			920			843	
Approach Delay, s/veh		30.3			26.6			20.7			19.8	
Approach LOS		C			C			C			B	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	9.3	36.3	16.7	15.5	9.6	36.0	11.0	21.2				
Change Period (Y+Rc), s	5.0	5.0	4.9	4.9	5.0	5.0	4.9	4.9				
Max Green Setting (Gmax), s	6.0	31.0	15.1	21.1	6.0	31.0	6.1	30.1				
Max Q Clear Time (g_c+I1), s	4.8	16.2	11.8	10.2	5.0	14.4	7.3	9.7				
Green Ext Time (p_c), s	0.0	1.3	0.0	0.4	0.0	1.1	0.0	0.4				
Intersection Summary												
HCM 7th Control Delay, s/veh			23.3									
HCM 7th LOS			C									

**LONG-RANGE (2050) CONDITIONS
WITHOUT PROJECT
LEVEL OF SERVICE
CALCULATIONS**

Intersection						
Int Delay, s/veh	2.2					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	252	8	44	289	25	67
Future Vol, veh/h	252	8	44	289	25	67
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	100	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	280	9	49	321	28	74

Major/Minor	Major1	Major2	Minor1		
Conflicting Flow All	0	0	289	0	703 284
Stage 1	-	-	-	-	284 -
Stage 2	-	-	-	-	419 -
Critical Hdwy	-	-	4.12	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	-	-	2.218	-	3.518 3.318
Pot Cap-1 Maneuver	-	-	1273	-	404 755
Stage 1	-	-	-	-	764 -
Stage 2	-	-	-	-	664 -
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1273	-	388 755
Mov Cap-2 Maneuver	-	-	-	-	388 -
Stage 1	-	-	-	-	764 -
Stage 2	-	-	-	-	638 -

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	0	1.05	12.22
HCM LOS			B

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	601	-	-	1273	-
HCM Lane V/C Ratio	0.17	-	-	0.038	-
HCM Ctrl Dly (s/v)	12.2	-	-	7.9	-
HCM Lane LOS	B	-	-	A	-
HCM 95th %tile Q(veh)	0.6	-	-	0.1	-

Intersection												
Int Delay, s/veh	0.3											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↶	↷		↶	↷		↶	↶↷		↶	↶↷	↶
Traffic Vol, veh/h	145	253	21	15	251	35	26	494	16	31	324	156
Future Vol, veh/h	145	253	21	15	251	35	26	494	16	31	324	156
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	150	-	-	150	-	-	500	-	-	500	-	125
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	158	275	23	16	273	38	28	537	17	34	352	170

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	881	1030	176	983	1191	277	522	0	0	554	0	0
Stage 1	420	420	-	602	602	-	-	-	-	-	-	-
Stage 2	461	611	-	381	589	-	-	-	-	-	-	-
Critical Hdwy	7.54	6.54	6.94	7.54	6.54	6.94	4.14	-	-	4.14	-	-
Critical Hdwy Stg 1	6.54	5.54	-	6.54	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.54	5.54	-	6.54	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.52	4.02	3.32	3.52	4.02	3.32	2.22	-	-	2.22	-	-
Pot Cap-1 Maneuver	241	~ 232	837	203	~ 186	720	1041	-	-	1012	-	-
Stage 1	582	588	-	453	487	-	-	-	-	-	-	-
Stage 2	550	483	-	613	494	-	-	-	-	-	-	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	~ 218	837	-	~ 175	720	1041	-	-	1012	-	-
Mov Cap-2 Maneuver	-	~ 218	-	-	~ 175	-	-	-	-	-	-	-
Stage 1	562	569	-	441	474	-	-	-	-	-	-	-
Stage 2	215	469	-	298	477	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Ctrl Dly, s/v			0.42	0.53
HCM LOS	-	-		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	EBLn2	WBLn1	WBLn2	SBL	SBT	SBR
Capacity (veh/h)	1041	-	-	-	-	-	-	1012	-	-
HCM Lane V/C Ratio	0.027	-	-	-	-	-	-	0.033	-	-
HCM Ctrl Dly (s/v)	8.6	-	-	-	-	-	-	8.7	-	-
HCM Lane LOS	A	-	-	-	-	-	-	A	-	-
HCM 95th %tile Q(veh)	0.1	-	-	-	-	-	-	0.1	-	-

Notes	
~: Volume exceeds capacity	\$: Delay exceeds 300s
+: Computation Not Defined	*: All major volume in platoon

HCM 7th Signalized Intersection Summary

3: S. Greeley Hwy & College Dr

12/19/2025



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	290	377	169	240	285	148	179	581	300	112	357	223
Future Volume (veh/h)	290	377	169	240	285	148	179	581	300	112	357	223
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	309	401	180	255	303	157	190	618	319	119	380	237
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	291	355	301	260	355	301	435	982	507	311	892	549
Arrive On Green	0.10	0.19	0.19	0.10	0.19	0.19	0.07	0.43	0.43	0.06	0.42	0.42
Sat Flow, veh/h	1781	1870	1585	1781	1870	1585	1781	2267	1170	1781	2114	1300
Grp Volume(v), veh/h	309	401	180	255	303	157	190	484	453	119	319	298
Grp Sat Flow(s),veh/h/ln	1781	1870	1585	1781	1870	1585	1781	1777	1660	1781	1777	1636
Q Serve(g_s), s	9.1	17.1	9.3	9.1	14.1	8.0	5.5	19.1	19.1	3.3	11.4	11.6
Cycle Q Clear(g_c), s	9.1	17.1	9.3	9.1	14.1	8.0	5.5	19.1	19.1	3.3	11.4	11.6
Prop In Lane	1.00		1.00	1.00		1.00	1.00		0.70	1.00		0.79
Lane Grp Cap(c), veh/h	291	355	301	260	355	301	435	769	719	311	750	691
V/C Ratio(X)	1.06	1.13	0.60	0.98	0.85	0.52	0.44	0.63	0.63	0.38	0.42	0.43
Avail Cap(c_a), veh/h	291	355	301	260	355	301	435	769	719	350	750	691
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	31.5	36.5	33.3	29.7	35.2	32.8	14.1	19.9	19.9	15.1	18.3	18.4
Incr Delay (d2), s/veh	69.7	87.3	2.3	50.0	17.0	0.8	0.3	3.9	4.2	0.3	1.8	2.0
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	7.6	16.2	3.6	7.9	7.8	3.0	1.9	7.8	7.3	1.2	4.5	4.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	101.2	123.7	35.6	79.7	52.2	33.5	14.4	23.8	24.0	15.4	20.1	20.3
LnGrp LOS	F	F	D	E	D	C	B	C	C	B	C	C
Approach Vol, veh/h		890			715			1127			736	
Approach Delay, s/veh		98.1			57.9			22.3			19.4	
Approach LOS		F			E			C			B	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	10.0	44.0	14.0	22.0	11.0	43.0	14.0	22.0				
Change Period (Y+Rc), s	5.0	5.0	4.9	4.9	5.0	5.0	4.9	4.9				
Max Green Setting (Gmax), s	7.0	37.0	9.1	17.1	6.0	38.0	9.1	17.1				
Max Q Clear Time (g_c+I1), s	5.3	21.1	11.1	19.1	7.5	13.6	11.1	16.1				
Green Ext Time (p_c), s	0.0	1.6	0.0	0.0	0.0	1.0	0.0	0.1				

Intersection Summary

HCM 7th Control Delay, s/veh	48.5
HCM 7th LOS	D

Notes

User approved pedestrian interval to be less than phase max green.

Intersection						
Int Delay, s/veh	2.4					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑		↑	↑	↑	
Traffic Vol, veh/h	183	11	76	161	15	36
Future Vol, veh/h	183	11	76	161	15	36
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	100	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	203	12	84	179	17	40

Major/Minor	Major1	Major2	Minor1			
Conflicting Flow All	0	0	216	0	557	209
Stage 1	-	-	-	-	209	-
Stage 2	-	-	-	-	348	-
Critical Hdwy	-	-	4.12	-	6.42	6.22
Critical Hdwy Stg 1	-	-	-	-	5.42	-
Critical Hdwy Stg 2	-	-	-	-	5.42	-
Follow-up Hdwy	-	-	2.218	-	3.518	3.318
Pot Cap-1 Maneuver	-	-	1354	-	491	831
Stage 1	-	-	-	-	826	-
Stage 2	-	-	-	-	715	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1354	-	461	831
Mov Cap-2 Maneuver	-	-	-	-	461	-
Stage 1	-	-	-	-	826	-
Stage 2	-	-	-	-	670	-

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	0	2.51	10.85
HCM LOS			B

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	672	-	-	1354	-
HCM Lane V/C Ratio	0.084	-	-	0.062	-
HCM Ctrl Dly (s/v)	10.8	-	-	7.8	-
HCM Lane LOS	B	-	-	A	-
HCM 95th %tile Q(veh)	0.3	-	-	0.2	-

Intersection												
Int Delay, s/veh	0.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↙	↘		↙	↘		↙	↕		↙	↕	↘
Traffic Vol, veh/h	145	153	19	10	159	20	15	606	11	27	577	102
Future Vol, veh/h	145	153	19	10	159	20	15	606	11	27	577	102
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	150	-	-	150	-	-	500	-	-	500	-	125
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	158	166	21	11	173	22	16	659	12	29	627	111

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	1134	1389	314	1153	1494	335	738	0	0	671	0	0
Stage 1	686	686	-	697	697	-	-	-	-	-	-	-
Stage 2	448	703	-	455	797	-	-	-	-	-	-	-
Critical Hdwy	7.54	6.54	6.94	7.54	6.54	6.94	4.14	-	-	4.14	-	-
Critical Hdwy Stg 1	6.54	5.54	-	6.54	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.54	5.54	-	6.54	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.52	4.02	3.32	3.52	4.02	3.32	2.22	-	-	2.22	-	-
Pot Cap-1 Maneuver	~ 157	~ 141	682	152	~ 122	660	864	-	-	916	-	-
Stage 1	404	446	-	397	441	-	-	-	-	-	-	-
Stage 2	559	438	-	554	397	-	-	-	-	-	-	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	~ 134	682	-	~ 116	660	864	-	-	916	-	-
Mov Cap-2 Maneuver	-	~ 134	-	-	~ 116	-	-	-	-	-	-	-
Stage 1	391	432	-	390	432	-	-	-	-	-	-	-
Stage 2	319	430	-	320	384	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Ctrl Dly, s/v			0.22	0.35
HCM LOS	-	-		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	EBLn2	WBLn1	WBLn2	SBL	SBT	SBR
Capacity (veh/h)	864	-	-	-	-	-	-	916	-	-
HCM Lane V/C Ratio	0.019	-	-	-	-	-	-	0.032	-	-
HCM Ctrl Dly (s/v)	9.2	-	-	-	-	-	-	9.1	-	-
HCM Lane LOS	A	-	-	-	-	-	-	A	-	-
HCM 95th %tile Q(veh)	0.1	-	-	-	-	-	-	0.1	-	-

Notes	
~: Volume exceeds capacity	\$: Delay exceeds 300s
+: Computation Not Defined	*: All major volume in platoon

HCM 7th Signalized Intersection Summary
 3: S. Greeley Hwy & College Dr

12/19/2025



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗	↘	↖	↗	↘	↖	↗↘		↖	↗↘	
Traffic Volume (veh/h)	226	297	179	335	305	177	154	519	348	161	575	240
Future Volume (veh/h)	226	297	179	335	305	177	154	519	348	161	575	240
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	235	309	186	349	318	184	160	541	362	168	599	250
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	333	352	298	410	542	459	292	714	477	273	856	357
Arrive On Green	0.07	0.19	0.19	0.17	0.29	0.29	0.07	0.35	0.35	0.07	0.35	0.35
Sat Flow, veh/h	1781	1870	1585	1781	1870	1585	1781	2039	1363	1781	2445	1019
Grp Volume(v), veh/h	235	309	186	349	318	184	160	472	431	168	435	414
Grp Sat Flow(s),veh/h/ln	1781	1870	1585	1781	1870	1585	1781	1777	1625	1781	1777	1687
Q Serve(g_s), s	6.1	14.2	9.6	13.8	12.9	8.3	5.1	20.8	20.8	5.4	18.7	18.7
Cycle Q Clear(g_c), s	6.1	14.2	9.6	13.8	12.9	8.3	5.1	20.8	20.8	5.4	18.7	18.7
Prop In Lane	1.00		1.00	1.00		1.00	1.00		0.84	1.00		0.60
Lane Grp Cap(c), veh/h	333	352	298	410	542	459	292	622	569	273	622	591
V/C Ratio(X)	0.71	0.88	0.62	0.85	0.59	0.40	0.55	0.76	0.76	0.62	0.70	0.70
Avail Cap(c_a), veh/h	333	446	378	410	636	539	292	622	569	273	622	591
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	25.9	35.0	33.1	24.1	26.9	25.3	19.1	25.5	25.5	19.9	24.8	24.8
Incr Delay (d2), s/veh	5.7	13.0	0.8	15.0	0.4	0.2	1.2	8.4	9.2	3.0	6.4	6.8
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.2	7.6	3.6	7.1	5.5	3.0	1.9	9.3	8.6	2.2	8.1	7.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	31.5	48.0	33.9	39.1	27.3	25.5	20.4	33.9	34.6	23.0	31.2	31.6
LnGrp LOS	C	D	C	D	C	C	C	C	C	C	C	C
Approach Vol, veh/h		730			851			1063			1017	
Approach Delay, s/veh		39.1			31.8			32.1			30.0	
Approach LOS		D			C			C			C	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	11.0	36.0	20.0	21.5	11.0	36.0	11.0	30.5				
Change Period (Y+Rc), s	5.0	5.0	4.9	4.9	5.0	5.0	4.9	4.9				
Max Green Setting (Gmax), s	6.0	31.0	15.1	21.1	6.0	31.0	6.1	30.1				
Max Q Clear Time (g_c+I1), s	7.4	22.8	15.8	16.2	7.1	20.7	8.1	14.9				
Green Ext Time (p_c), s	0.0	1.3	0.0	0.4	0.0	1.3	0.0	0.6				
Intersection Summary												
HCM 7th Control Delay, s/veh			32.8									
HCM 7th LOS			C									

HCM 7th Signalized Intersection Summary
 2: S. Greeley Hwy & Wallick Rd

12/19/2025



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↶	↷		↶	↷		↶	↷		↶	↷	↶
Traffic Volume (veh/h)	145	253	21	15	251	35	26	494	16	31	324	156
Future Volume (veh/h)	145	253	21	15	251	35	26	494	16	31	324	156
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	158	275	23	16	273	38	28	537	17	34	352	170
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	300	613	51	312	327	46	433	1416	45	391	1443	644
Arrive On Green	0.09	0.36	0.36	0.20	0.20	0.20	0.02	0.40	0.40	0.03	0.41	0.41
Sat Flow, veh/h	1781	1702	142	1081	1606	224	1781	3516	111	1781	3554	1585
Grp Volume(v), veh/h	158	0	298	16	0	311	28	271	283	34	352	170
Grp Sat Flow(s),veh/h/ln	1781	0	1845	1081	0	1830	1781	1777	1850	1781	1777	1585
Q Serve(g_s), s	5.4	0.0	9.7	0.9	0.0	12.8	0.7	8.4	8.4	0.9	5.1	5.6
Cycle Q Clear(g_c), s	5.4	0.0	9.7	0.9	0.0	12.8	0.7	8.4	8.4	0.9	5.1	5.6
Prop In Lane	1.00		0.08	1.00		0.12	1.00		0.06	1.00		1.00
Lane Grp Cap(c), veh/h	300	0	664	312	0	373	433	715	745	391	1443	644
V/C Ratio(X)	0.53	0.00	0.45	0.05	0.00	0.83	0.06	0.38	0.38	0.09	0.24	0.26
Avail Cap(c_a), veh/h	363	0	896	410	0	538	483	715	745	435	1443	644
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	22.5	0.0	19.1	25.2	0.0	29.9	13.1	16.5	16.5	13.5	15.3	15.5
Incr Delay (d2), s/veh	1.4	0.0	0.5	0.1	0.0	7.5	0.1	1.5	1.5	0.1	0.4	1.0
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.3	0.0	4.0	0.2	0.0	6.2	0.3	3.2	3.4	0.3	1.9	2.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	23.9	0.0	19.6	25.3	0.0	37.4	13.2	18.0	18.0	13.6	15.7	16.5
LnGrp LOS	C		B	C		D	B	B	B	B	B	B
Approach Vol, veh/h		456			327			582			556	
Approach Delay, s/veh		21.1			36.8			17.7			15.8	
Approach LOS		C			D			B			B	
Timer - Assigned Phs	1	2		4	5	6	7	8				
Phs Duration (G+Y+Rc), s	7.1	38.0		33.2	6.8	38.3	12.2	20.9				
Change Period (Y+Rc), s	5.0	6.5		5.0	5.0	6.5	5.0	5.0				
Max Green Setting (Gmax), s	4.0	31.5		38.0	4.0	31.5	10.0	23.0				
Max Q Clear Time (g_c+I1), s	2.9	10.4		11.7	2.7	7.6	7.4	14.8				
Green Ext Time (p_c), s	0.0	2.8		1.8	0.0	2.5	0.1	1.2				
Intersection Summary												
HCM 7th Control Delay, s/veh			21.2									
HCM 7th LOS			C									

HCM 7th Signalized Intersection Summary

2: S. Greeley Hwy & Wallick Rd

12/19/2025



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↶	↷		↶	↷		↶	↷		↶	↷	↶
Traffic Volume (veh/h)	145	153	19	10	159	20	15	606	11	27	577	102
Future Volume (veh/h)	145	153	19	10	159	20	15	606	11	27	577	102
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	158	166	21	11	173	22	16	659	12	29	627	111
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	428	629	80	367	311	40	258	966	18	266	998	445
Arrive On Green	0.10	0.39	0.39	0.19	0.19	0.19	0.02	0.27	0.27	0.03	0.28	0.28
Sat Flow, veh/h	1781	1627	206	1196	1626	207	1781	3571	65	1781	3554	1585
Grp Volume(v), veh/h	158	0	187	11	0	195	16	328	343	29	627	111
Grp Sat Flow(s),veh/h/ln	1781	0	1833	1196	0	1833	1781	1777	1859	1781	1777	1585
Q Serve(g_s), s	3.6	0.0	3.6	0.4	0.0	5.0	0.3	8.6	8.6	0.6	8.0	2.8
Cycle Q Clear(g_c), s	3.6	0.0	3.6	0.4	0.0	5.0	0.3	8.6	8.6	0.6	8.0	2.8
Prop In Lane	1.00		0.11	1.00		0.11	1.00		0.03	1.00		1.00
Lane Grp Cap(c), veh/h	428	0	709	367	0	351	258	481	503	266	998	445
V/C Ratio(X)	0.37	0.00	0.26	0.03	0.00	0.56	0.06	0.68	0.68	0.11	0.63	0.25
Avail Cap(c_a), veh/h	593	0	1302	644	0	774	401	1075	1124	390	2149	959
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	14.7	0.0	10.9	17.2	0.0	19.1	13.6	17.0	17.0	13.8	16.4	14.5
Incr Delay (d2), s/veh	0.5	0.0	0.2	0.0	0.0	1.4	0.1	1.7	1.6	0.2	0.7	0.3
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.3	0.0	1.3	0.1	0.0	2.1	0.1	3.0	3.1	0.2	2.6	0.9
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	15.2	0.0	11.1	17.2	0.0	20.4	13.7	18.7	18.6	14.0	17.0	14.8
LnGrp LOS	B		B	B		C	B	B	B	B	B	B
Approach Vol, veh/h		345			206			687			767	
Approach Delay, s/veh		13.0			20.3			18.6			16.6	
Approach LOS		B			C			B			B	
Timer - Assigned Phs	1	2		4	5	6	7	8				
Phs Duration (G+Y+Rc), s	6.4	20.6		25.1	5.8	21.1	10.2	15.0				
Change Period (Y+Rc), s	5.0	6.5		5.0	5.0	6.5	5.0	5.0				
Max Green Setting (Gmax), s	5.0	31.5		37.0	5.0	31.5	10.0	22.0				
Max Q Clear Time (g_c+1), s	2.6	10.6		5.6	2.3	10.0	5.6	7.0				
Green Ext Time (p_c), s	0.0	3.5		1.1	0.0	4.1	0.2	0.9				
Intersection Summary												
HCM 7th Control Delay, s/veh				17.0								
HCM 7th LOS				B								

**LONG-RANGE (2050) CONDITIONS
WITH PROJECT
LEVEL OF SERVICE
CALCULATIONS**

Intersection						
Int Delay, s/veh	2					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	401	14	44	471	32	67
Future Vol, veh/h	401	14	44	471	32	67
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	100	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	446	16	49	523	36	74

Major/Minor	Major1	Major2	Minor1			
Conflicting Flow All	0	0	461	0	1074	453
Stage 1	-	-	-	-	453	-
Stage 2	-	-	-	-	621	-
Critical Hdwy	-	-	4.12	-	6.42	6.22
Critical Hdwy Stg 1	-	-	-	-	5.42	-
Critical Hdwy Stg 2	-	-	-	-	5.42	-
Follow-up Hdwy	-	-	2.218	-	3.518	3.318
Pot Cap-1 Maneuver	-	-	1100	-	243	607
Stage 1	-	-	-	-	640	-
Stage 2	-	-	-	-	536	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1100	-	232	607
Mov Cap-2 Maneuver	-	-	-	-	232	-
Stage 1	-	-	-	-	640	-
Stage 2	-	-	-	-	512	-

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	0	0.72	17.42
HCM LOS			C

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	399	-	-	1100	-
HCM Lane V/C Ratio	0.276	-	-	0.044	-
HCM Ctrl Dly (s/v)	17.4	-	-	8.4	-
HCM Lane LOS	C	-	-	A	-
HCM 95th %tile Q(veh)	1.1	-	-	0.1	-

HCM 7th Signalized Intersection Summary

2: S. Greeley Hwy & Wallick Rd

12/19/2025



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗		↖	↕	↗	↖	↕	↗
Traffic Volume (veh/h)	244	293	31	15	300	35	38	494	16	31	324	274
Future Volume (veh/h)	244	293	31	15	300	35	38	494	16	31	324	274
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	265	318	34	16	326	38	41	537	17	34	352	298
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	331	670	72	318	372	43	374	1320	42	356	1323	590
Arrive On Green	0.12	0.40	0.40	0.23	0.23	0.23	0.03	0.38	0.38	0.03	0.37	0.37
Sat Flow, veh/h	1781	1661	178	1029	1644	192	1781	3516	111	1781	3554	1585
Grp Volume(v), veh/h	265	0	352	16	0	364	41	271	283	34	352	298
Grp Sat Flow(s),veh/h/ln	1781	0	1838	1029	0	1836	1781	1777	1850	1781	1777	1585
Q Serve(g_s), s	9.7	0.0	12.0	1.0	0.0	16.2	1.2	9.5	9.5	1.0	5.8	12.3
Cycle Q Clear(g_c), s	9.7	0.0	12.0	1.0	0.0	16.2	1.2	9.5	9.5	1.0	5.8	12.3
Prop In Lane	1.00		0.10	1.00		0.10	1.00		0.06	1.00		1.00
Lane Grp Cap(c), veh/h	331	0	742	318	0	416	374	667	695	356	1323	590
V/C Ratio(X)	0.80	0.00	0.47	0.05	0.00	0.88	0.11	0.41	0.41	0.10	0.27	0.51
Avail Cap(c_a), veh/h	331	0	825	365	0	499	406	667	695	394	1323	590
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	23.3	0.0	18.6	25.7	0.0	31.6	15.7	19.5	19.5	16.0	18.5	20.5
Incr Delay (d2), s/veh	13.0	0.0	0.5	0.1	0.0	14.1	0.1	1.8	1.8	0.1	0.5	3.1
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	5.1	0.0	4.9	0.3	0.0	8.6	0.4	3.8	4.0	0.4	2.2	4.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	36.4	0.0	19.1	25.8	0.0	45.7	15.9	21.3	21.3	16.1	19.0	23.6
LnGrp LOS	D		B	C		D	B	C	C	B	B	C
Approach Vol, veh/h		617			380			595			684	
Approach Delay, s/veh		26.5			44.8			20.9			20.9	
Approach LOS		C			D			C			C	
Timer - Assigned Phs	1	2		4	5	6	7	8				
Phs Duration (G+Y+Rc), s	7.2	38.3		39.2	7.5	38.0	15.0	24.2				
Change Period (Y+Rc), s	5.0	6.5		5.0	5.0	6.5	5.0	5.0				
Max Green Setting (Gmax), s	4.0	31.5		38.0	4.0	31.5	10.0	23.0				
Max Q Clear Time (g_c+I1), s	3.0	11.5		14.0	3.2	14.3	11.7	18.2				
Green Ext Time (p_c), s	0.0	2.7		2.2	0.0	2.8	0.0	1.0				
Intersection Summary												
HCM 7th Control Delay, s/veh			26.4									
HCM 7th LOS			C									

HCM 7th Signalized Intersection Summary
 3: S. Greeley Hwy & College Dr

12/19/2025



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↙	↑	↗	↙	↑	↗	↙	↑↕		↙	↑↕	
Traffic Volume (veh/h)	290	377	169	289	285	148	179	632	340	112	419	223
Future Volume (veh/h)	290	377	169	289	285	148	179	632	340	112	419	223
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No		No		No		No		No		No
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	309	401	180	307	303	157	190	672	362	119	446	237
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	291	355	301	260	355	301	409	966	520	282	949	500
Arrive On Green	0.10	0.19	0.19	0.10	0.19	0.19	0.07	0.43	0.43	0.06	0.42	0.42
Sat Flow, veh/h	1781	1870	1585	1781	1870	1585	1781	2230	1201	1781	2249	1185
Grp Volume(v), veh/h	309	401	180	307	303	157	190	535	499	119	352	331
Grp Sat Flow(s),veh/h/ln	1781	1870	1585	1781	1870	1585	1781	1777	1654	1781	1777	1657
Q Serve(g_s), s	9.1	17.1	9.3	9.1	14.1	8.0	5.5	22.0	22.0	3.3	12.8	13.0
Cycle Q Clear(g_c), s	9.1	17.1	9.3	9.1	14.1	8.0	5.5	22.0	22.0	3.3	12.8	13.0
Prop In Lane	1.00		1.00	1.00		1.00	1.00		0.73	1.00		0.72
Lane Grp Cap(c), veh/h	291	355	301	260	355	301	409	769	716	282	750	700
V/C Ratio(X)	1.06	1.13	0.60	1.18	0.85	0.52	0.46	0.70	0.70	0.42	0.47	0.47
Avail Cap(c_a), veh/h	291	355	301	260	355	301	409	769	716	321	750	700
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	31.5	36.5	33.3	30.0	35.2	32.8	14.4	20.7	20.7	16.0	18.7	18.8
Incr Delay (d2), s/veh	69.7	87.3	2.3	113.5	17.0	0.8	0.3	5.2	5.5	0.4	2.1	2.3
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	7.6	16.2	3.6	12.6	7.8	3.0	1.9	9.1	8.5	1.2	5.1	4.9
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	101.2	123.7	35.6	143.5	52.2	33.5	14.7	25.9	26.2	16.4	20.8	21.1
LnGrp LOS	F	F	D	F	D	C	B	C	C	B	C	C
Approach Vol, veh/h		890			767			1224			802	
Approach Delay, s/veh		98.1			84.9			24.3			20.3	
Approach LOS		F			F			C			C	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	40.0	44.0	14.0	22.0	11.0	43.0	14.0	22.0				
Change Period (Y+Rc), s	5.0	5.0	4.9	4.9	5.0	5.0	4.9	4.9				
Max Green Setting (Gmax), s	37.0	37.0	9.1	17.1	6.0	38.0	9.1	17.1				
Max Q Clear Time (g_c+1/3), s	24.0	24.0	11.1	19.1	7.5	15.0	11.1	16.1				
Green Ext Time (p_c), s	0.0	1.7	0.0	0.0	0.0	1.1	0.0	0.1				

Intersection Summary												
HCM 7th Control Delay, s/veh											53.9	
HCM 7th LOS											D	

Notes
 User approved pedestrian interval to be less than phase max green.

Intersection						
Int Delay, s/veh	6.7					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑	↑		↑	
Traffic Vol, veh/h	0	320	270	0	179	53
Future Vol, veh/h	0	320	270	0	179	53
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	90	90	90	90	70	70
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	356	300	0	256	76

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	-	0	-	0	656 300
Stage 1	-	-	-	-	300 -
Stage 2	-	-	-	-	356 -
Critical Hdwy	-	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	-	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	0	-	-	0	431 740
Stage 1	0	-	-	0	752 -
Stage 2	0	-	-	0	709 -
Platoon blocked, %		-	-		
Mov Cap-1 Maneuver	-	-	-	-	431 740
Mov Cap-2 Maneuver	-	-	-	-	529 -
Stage 1	-	-	-	-	752 -
Stage 2	-	-	-	-	709 -

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	0	0	19.96
HCM LOS			C

Minor Lane/Major Mvmt	EBT	WBT	SBLn1
Capacity (veh/h)	-	-	566
HCM Lane V/C Ratio	-	-	0.586
HCM Ctrl Dly (s/v)	-	-	20
HCM Lane LOS	-	-	C
HCM 95th %tile Q(veh)	-	-	3.8

Intersection						
Int Delay, s/veh	2					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	261	14	76	227	18	36
Future Vol, veh/h	261	14	76	227	18	36
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	100	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	90	90	90	90	90	90
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	290	16	84	252	20	40

Major/Minor	Major1	Major2	Minor1		
Conflicting Flow All	0	0	306	0	719 298
Stage 1	-	-	-	-	298 -
Stage 2	-	-	-	-	421 -
Critical Hdwy	-	-	4.12	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	-	-	2.218	-	3.518 3.318
Pot Cap-1 Maneuver	-	-	1255	-	395 742
Stage 1	-	-	-	-	753 -
Stage 2	-	-	-	-	662 -
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1255	-	369 742
Mov Cap-2 Maneuver	-	-	-	-	369 -
Stage 1	-	-	-	-	753 -
Stage 2	-	-	-	-	618 -

Approach	EB	WB	NB
HCM Ctrl Dly, s/v	0	2.03	12.28
HCM LOS			B

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	555	-	-	1255	-
HCM Lane V/C Ratio	0.108	-	-	0.067	-
HCM Ctrl Dly (s/v)	12.3	-	-	8.1	-
HCM Lane LOS	B	-	-	A	-
HCM 95th %tile Q(veh)	0.4	-	-	0.2	-

HCM 7th Signalized Intersection Summary
 2: S. Greeley Hwy & Wallick Rd

12/19/2025



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↶	↷		↶	↷		↶	↷		↶	↷	↷
Traffic Volume (veh/h)	196	174	24	10	177	20	19	606	11	27	577	145
Future Volume (veh/h)	196	174	24	10	177	20	19	606	11	27	577	145
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	213	189	26	11	192	22	21	659	12	29	627	158
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	449	650	89	347	303	35	249	952	17	256	969	432
Arrive On Green	0.13	0.40	0.40	0.18	0.18	0.18	0.02	0.27	0.27	0.03	0.27	0.27
Sat Flow, veh/h	1781	1609	221	1166	1648	189	1781	3571	65	1781	3554	1585
Grp Volume(v), veh/h	213	0	215	11	0	214	21	328	343	29	627	158
Grp Sat Flow(s),veh/h/ln	1781	0	1831	1166	0	1836	1781	1777	1859	1781	1777	1585
Q Serve(g_s), s	5.1	0.0	4.3	0.4	0.0	5.9	0.5	9.0	9.0	0.6	8.5	4.4
Cycle Q Clear(g_c), s	5.1	0.0	4.3	0.4	0.0	5.9	0.5	9.0	9.0	0.6	8.5	4.4
Prop In Lane	1.00		0.12	1.00		0.10	1.00		0.03	1.00		1.00
Lane Grp Cap(c), veh/h	449	0	739	347	0	337	249	474	496	256	969	432
V/C Ratio(X)	0.47	0.00	0.29	0.03	0.00	0.63	0.08	0.69	0.69	0.11	0.65	0.37
Avail Cap(c_a), veh/h	549	0	1246	605	0	743	377	1030	1077	373	2060	919
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	15.0	0.0	10.9	18.3	0.0	20.5	14.4	17.9	17.9	14.6	17.5	16.0
Incr Delay (d2), s/veh	0.8	0.0	0.2	0.0	0.0	2.0	0.1	1.8	1.7	0.2	0.7	0.5
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.9	0.0	1.5	0.1	0.0	2.5	0.2	3.2	3.3	0.2	2.8	1.5
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	15.8	0.0	11.2	18.3	0.0	22.5	14.6	19.7	19.7	14.8	18.2	16.5
LnGrp LOS	B		B	B		C	B	B	B	B	B	B
Approach Vol, veh/h		428			225			692			814	
Approach Delay, s/veh		13.5			22.3			19.6			17.7	
Approach LOS		B			C			B			B	
Timer - Assigned Phs	1	2		4	5	6	7	8				
Phs Duration (G+Y+Rc), s	6.4	21.0		26.9	6.1	21.3	12.0	15.0				
Change Period (Y+Rc), s	5.0	6.5		5.0	5.0	6.5	5.0	5.0				
Max Green Setting (Gmax), s	5.0	31.5		37.0	5.0	31.5	10.0	22.0				
Max Q Clear Time (g_c+I1), s	2.6	11.0		6.3	2.5	10.5	7.1	7.9				
Green Ext Time (p_c), s	0.0	3.5		1.3	0.0	4.2	0.2	1.0				
Intersection Summary												
HCM 7th Control Delay, s/veh											17.9	
HCM 7th LOS											B	

HCM 7th Signalized Intersection Summary
 3: S. Greeley Hwy & College Dr

12/19/2025



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↘	↑	↗	↘	↑	↗	↘	↕	↗	↘	↕	↗
Traffic Volume (veh/h)	226	297	179	353	305	177	154	546	369	161	597	240
Future Volume (veh/h)	226	297	179	353	305	177	154	546	369	161	597	240
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Lane Width Adj.	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No		No		No		No		No		No
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	235	309	186	368	318	184	160	569	384	168	622	250
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	333	352	298	410	542	459	286	711	480	259	866	348
Arrive On Green	0.07	0.19	0.19	0.17	0.29	0.29	0.07	0.35	0.35	0.07	0.35	0.35
Sat Flow, veh/h	1781	1870	1585	1781	1870	1585	1781	2030	1370	1781	2474	994
Grp Volume(v), veh/h	235	309	186	368	318	184	160	498	455	168	447	425
Grp Sat Flow(s),veh/h/ln	1781	1870	1585	1781	1870	1585	1781	1777	1624	1781	1777	1691
Q Serve(g_s), s	6.1	14.2	9.6	14.8	12.9	8.3	5.1	22.4	22.4	5.4	19.3	19.3
Cycle Q Clear(g_c), s	6.1	14.2	9.6	14.8	12.9	8.3	5.1	22.4	22.4	5.4	19.3	19.3
Prop In Lane	1.00		1.00	1.00		1.00	1.00		0.84	1.00		0.59
Lane Grp Cap(c), veh/h	333	352	298	410	542	459	286	622	568	259	622	592
V/C Ratio(X)	0.71	0.88	0.62	0.90	0.59	0.40	0.56	0.80	0.80	0.65	0.72	0.72
Avail Cap(c_a), veh/h	333	446	378	410	636	539	286	622	568	259	622	592
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	25.9	35.0	33.1	24.4	26.9	25.3	19.3	26.0	26.0	20.5	25.0	25.0
Incr Delay (d2), s/veh	5.7	13.0	0.8	21.5	0.4	0.2	1.5	10.4	11.3	4.4	7.0	7.3
Initial Q Delay(d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	2.2	7.6	3.6	8.2	5.5	3.0	2.0	10.2	9.5	2.3	8.5	8.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d), s/veh	31.5	48.0	33.9	45.9	27.3	25.5	20.8	36.4	37.3	24.9	32.0	32.3
LnGrp LOS	C	D	C	D	C	C	C	D	D	C	C	C
Approach Vol, veh/h		730			870			1113			1040	
Approach Delay, s/veh		39.1			34.8			34.5			31.0	
Approach LOS		D			C			C			C	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	1.0	36.0	20.0	21.5	11.0	36.0	11.0	30.5				
Change Period (Y+Rc), s	5.0	5.0	4.9	4.9	5.0	5.0	4.9	4.9				
Max Green Setting (Gmax), s	31.0	15.1	21.1	6.0	31.0	6.1	30.1					
Max Q Clear Time (g_c+17), s	24.4	16.8	16.2	7.1	21.3	8.1	14.9					
Green Ext Time (p_c), s	0.0	1.2	0.0	0.4	0.0	1.3	0.0	0.6				
Intersection Summary												
HCM 7th Control Delay, s/veh											34.5	
HCM 7th LOS											C	

Intersection						
Int Delay, s/veh	3.7					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑	↑		↑	
Traffic Vol, veh/h	0	184	155	0	93	28
Future Vol, veh/h	0	184	155	0	93	28
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	90	90	90	90	70	70
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	204	172	0	133	40

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	-	0	-	0	377 172
Stage 1	-	-	-	-	172 -
Stage 2	-	-	-	-	204 -
Critical Hdwy	-	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	-	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	0	-	-	0	625 871
Stage 1	0	-	-	0	858 -
Stage 2	0	-	-	0	830 -
Platoon blocked, %		-	-		
Mov Cap-1 Maneuver	-	-	-	-	625 871
Mov Cap-2 Maneuver	-	-	-	-	670 -
Stage 1	-	-	-	-	858 -
Stage 2	-	-	-	-	830 -

Approach	EB	WB	SB
HCM Ctrl Dly, s/v	0	0	11.72
HCM LOS			B

Minor Lane/Major Mvmt	EBT	WBT	SBLn1
Capacity (veh/h)	-	-	708
HCM Lane V/C Ratio	-	-	0.244
HCM Ctrl Dly (s/v)	-	-	11.7
HCM Lane LOS	-	-	B
HCM 95th %tile Q(veh)	-	-	1

AGENCY REVIEW #3

Permit Notes

Permit Number: PZ-26-00024

Parcel Number: 13661730501300

Submitted: 03/18/2026

Applicant: HANSEN, MICHEAL SHANE
Owner: LARAMIE COUNTY SCHOOL DIST NO 1
Project Description: COMBINE TWO LOTS INTO ONE

Site Address: 400 W WALLICK RD
 Cheyenne, WY 82007

Technically Complete: 03/18/2026
Approved:
Issued:

<u>Begin Date</u>	<u>End Date</u>	<u>Permit Area</u>	<u>Subject</u>	<u>Note Type</u>	<u>Note Text</u>	<u>Created By</u>
12/26/2024	03/14/2025	Parcels	13661730501300	POP-UP	plan review only is on PR-24-00005 for LCSD#1 Afflerbach Elementary Lighting Upgrade. When we receive building permit please check to make sure plans are the same and delete plan review only. Please ask SC for help.	SONIA.CHRISTEN SEN@LARAMIECO UNTYWY.GOV
01/06/2025	02/28/2025	Parcels	13661730501300	POP-UP	plan review only is on PR-25-00001 for LCSD#1 Afflerbach Elementary Boiler Replacement. When we receive building permit please check to make sure plans are the same and delete plan review only. Please ask SC for help.	BEAU.SAINDON@L ARAMIECOUNTYW Y.GOV
03/18/2026		Application	PZ-26-00024	GENERAL	BOCC Public Hearing May 5, 2026. Letters sent 3.19.26, legal ad published 3.25.26.	CATHERINE.CUND ALL@LARAMIECO UNTYWY.GOV
03/19/2026		Workflow	COUNTY REAL ESTATE OFFICE REVIEW	GENERAL	No Comments.	RACHEL.TRUDEA U@LARAMIECOUN TYWY.GOV
03/19/2026		Workflow	GIS REVIEW	GENERAL	It is difficult to read and determine all the small adjustments that are being done along the southern border of the property. Would it be possible to do an inset of the specific area (or something similar) so that is clearly marked what is being dedicated to ROW.	CAMBIA.MCCOLLO M@LARAMIECOU NTYWY.GOV
03/25/2026		Application	PZ-26-00024	GENERAL	No concerns with this proposal.	WYGAMEFISHDEP T@LARAMIECOUN TYWY.GOV
03/26/2026		Workflow	FIRE DISTRICT REVIEW	GENERAL	No comments at this time	JEFF.SHAFER@L ARAMIECOUNTYW Y.GOV
03/27/2026		Application	PZ-26-00024	GENERAL	1. Please change the 40' Road Reservation to Dedicate 40' right-of-Way for Division Avenue.	CHRISTOPHER.YA NEY@LARAMIECO UNTYWY.GOV
03/27/2026		Workflow	COUNTY ASSESSOR REVIEW	GENERAL	No comments.	CINDY.KEMIVES@ LARAMIECOUNTY WY.GOV
03/30/2026		Application	PZ-26-00024	GENERAL	Appears District easements will remain in place. There are no comments on this piece of property development.	SCOTT.SPRAKTIE S@LARAMIECOUN TYWY.GOV
03/30/2026		Application	PZ-26-00024	GENERAL	No comments	MATTHEW.BUTLE R@LARAMIECOUN TYWY.GOV

Permit Notes

03/30/2026	Workflow	ENGINEERS REVIEW	GENERAL	<p>1st Review</p> <p>Engineers Comments:</p> <ol style="list-style-type: none"> 1.In the title block, it is called out as a "Small Replat", however, under the LCLUR it is classified as a "Modest Plat". 2.There are two 55.00' x 10.00' and one 65.00' x 10.00' blocks shown along the southern property line. Please indicate on the plat what these are for. Dedicated ROW? 3.Between the 55.00' x 10.00' blocks and the 65.00' x 10.00' block there is a label indicating "Dedicated Right of Way" and dimension leaders, but no dimension is shown and the lines are too bold/wide to see any space between them to get an accurate measurement. Please include the measurement. 4.The 40' Road Reservation for Division Ave needs to be changed on this plat and dedicated as a public right-of-way. 5.A Drainage Report was not submitted. However, the County has been working with the design team for the new school regarding drainage and a report will be submitted with the Site Plan application submittal. <p>Surveyors Comments:</p> <ol style="list-style-type: none"> 1.The northeast center line end of the 20' SEWER & WATER EASEMENT B00K 690 -PAGE 525 is shown as 15.25' north of the point of intersection in the north boundary of LOT 1 BLOCK 1. Is the easterly portion of the north boundary of the 9' SEWER & DRAINAGE RESERVATION BOOK 1106 PAGE 1176 located on the boundary line of LOT 1 BLOCK 1 of this subdivision. Please clarify on the plat drawing. 	SCOTT.LARSON@LARAMIECOUNTY.WY.GOV
03/31/2026	Workflow	PUBLIC WORKS REVIEW	GENERAL	<ol style="list-style-type: none"> 1. All comments from the review engineer and surveyor shall be addressed and resolved appropriately. 2. Future infrastructure upgrades may be required with this development. This could include but is not limited to access, right-of-way (road construction), re-vegetation and signage. Permit applications through Public Works will be required at the time of development. 	MOLLY.BENNETT@LARAMIECOUNTY.WY.GOV
04/01/2026	Application	PZ-26-00024	GENERAL	WAPA has no conflict with this project.	ROGERS@LARAMIECOUNTY.WY.GOV
04/03/2026	Application	PZ-26-00024	GENERAL	<p>Plat</p> <ol style="list-style-type: none"> 1.Dedicate 40 feet of right-of-way along the west side of this site for Division Ave. <p>Traffic Study</p> <ol style="list-style-type: none"> 1. Executive Summary / Required Improvements (Pages 15) Clearly distinguish required mitigation measures from future monitoring items. The improvements required shall be clearly identified. 2. Future Growth Assumptions (Pages 67) Provide the basis for long-range roadway assumptions and annual growth rates, including consistency with adopted County and MPO plans. 3. Methodology (Pages 1114) 	CHRISTOPHER.YANEY@LARAMIECOUNTY.WY.GOV

Permit Notes

Clarify the analysis software, HCM methodology, and school peak hour assumptions used in the LOS and queue analyses.

4. Existing Conditions / Safety (Pages 1520)
 Include observed operating speed data and expand the safety discussion to address school pickup/drop-off, pedestrian activity, and bus circulation.

5. Queueing / On-Site Circulation (Page 20)
 Provide a formal queue analysis for pickup and dismissal operations, including maximum queue length, storage provided, and spillback potential to Wallick Road.

6. On-Street Parking (Page 20)
 Provide a formal recommendation for curbside parking restrictions along Wallick Road during school pickup and dismissal periods.

7. Turn Lane Improvement (Pages 2223)
 The westbound left-turn lane at Avenue B-2 / Wallick Road shall be identified as a required mitigation measure. Include storage and taper lengths based on applicable design standards.

8. Bus Access / Division Avenue (Pages 6, 10, and Exhibit 2 Page 13)
 The Traffic Impact Study identifies the western most access nearest the intersection of Wallick Road and Division Avenue for the bus load/unload area.
 To improve traffic operations and reduce conflicts on Wallick Road, all bus access(es) shall be placed onto Division Avenue, and a portion of Division Avenue shall be constructed to accommodate school bus traffic, including appropriate pavement section, turning radii, and ingress/egress movements.

9. Signal Warrant Analysis (Pages 2428)
 Provide signal warrant analysis for opening year and future build conditions, including school-related pedestrian crossing activity.

10. Improvement Exhibit
 Add an exhibit showing all recommended on-site and off-site traffic improvements associated with the development.

04/03/2026	Workflow	ENGINEERS REVIEW	GENERAL	2nd Review - The 40' Road Reservation for Division Ave shown to be vacated needs to be changed on this plat and dedicated as a public right-of-way to allow for the new/proposed Division Avenue to line up with the ROW for Division south of West Wallick Road and the intersection of Division and Wallick can be constructed properly in the future.	SCOTT.LARSON@LARAMIECOUNTY.WY.GOV
04/07/2026	Workflow	PUBLIC WORKS REVIEW	GENERAL	1. All comments from the review engineer and the Cheyenne MPO shall be addressed and resolved appropriately.	MOLLY.BENNETT@LARAMIECOUNTY.WY.GOV

Permit Notes

04/09/2026	Workflow	ENGINEERS REVIEW	GENERAL	2nd Review - Previous comments have been adequately addressed on the revised plat drawing. No further comments at this time. ***3RD REVIEW**	SCOTT.LARSON@LARAMIECOUNTY WY.GOV
04/13/2026	Application	PZ-26-00024	GENERAL	Primary documents	SONNY.HOOPS@LARAMIECOUNTY WY.GOV
04/13/2026	Workflow	PUBLIC WORKS REVIEW	GENERAL	1. no further comments on the plat. 2. Comments regarding the traffic study shall be addressed with the site plan application.	MOLLY.BENNETT@LARAMIECOUNTY WY.GOV
04/15/2026	Application	PZ-26-00024	GENERAL	1. Right-of-way has been addressed on plat. 2. Comments regarding the traffic study shall be addressed with the site plan application.	CHRISTOPHER.YANEY@LARAMIECOUNTY WY.GOV

AGENCY REVIEW #2

Permit Notes

APPLICANT RESPONSE

Permit Number: PZ-26-00024

Parcel Number: 13661730501300

Submitted: 03/18/2026

Applicant: HANSEN, MICHEAL SHANE
Owner: LARAMIE COUNTY SCHOOL DIST NO 1
Project Description: COMBINE TWO LOTS INTO ONE

Site Address: 400 W WALLICK RD
 Cheyenne, WY 82007

Technically Complete: 03/18/2026
Approved:
Issued:

<u>Begin Date</u>	<u>End Date</u>	<u>Permit Area</u>	<u>Subject</u>	<u>Note Type</u>	<u>Note Text</u>	<u>Created By</u>
12/26/2024	03/14/2025	Parcels	13661730501300	POP-UP	plan review only is on PR-24-00005 for LCSD#1 Afflerbach Elementary Lighting Upgrade. When we receive building permit please check to make sure plans are the same and delete plan review only. Please ask SC for help.	SONIA.CHRISTEN SEN@LARAMIECO UNTYWY.GOV
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03/19/2026		Workflow	GIS REVIEW	GENERAL	It is difficult to read and determine all the small adjustments that are being done along the southern border of the property. Would it be possible to do an inset of the specific area (or something similar) so that is clearly marked what is being dedicated to ROW.	CAMBIA.MCCOLLO M@LARAMIECOU NTYWY.GOV
03/25/2026		Application	PZ-26-00024	GENERAL	No concerns with this proposal.	WYGAMEFISHDEP T@LARAMIECOUN TYWY.GOV
03/26/2026		Workflow	FIRE DISTRICT REVIEW	GENERAL	No comments at this time	JEFF.SHAFER@L ARAMIECOUNTYW Y.GOV
03/27/2026		Application	PZ-26-00024	GENERAL	1. Please change the 40' Road Reservation to Dedicate 40' right-of-Way for Division Avenue.	CHRISTOPHER.YA NEY@LARAMIECO UNTYWY.GOV
03/27/2026		Workflow	COUNTY ASSESSOR REVIEW	GENERAL	No comments.	CINDY.KEMIVES@ LARAMIECOUNTY WY.GOV
03/30/2026		Application	PZ-26-00024	GENERAL	Appears District easements will remain in place. There are no comments on this piece of property development.	SCOTT.SPRAKTIE S@LARAMIECOUN TYWY.GOV
03/30/2026		Application	PZ-26-00024	GENERAL	No comments	MATTHEW.BUTLE R@LARAMIECOUN TYWY.GOV

Permit Notes

03/30/2026	Workflow	ENGINEERS REVIEW	GENERAL	<p>1st Review</p> <p>Engineers Comments:</p> <p>1.In the title block, it is called out as a "Small Replat", however, under the LCLUR it is classified as a "Modest Plat".</p> <p>2.There are two 55.00' x 10.00' and one 65.00' x 10.00' blocks shown along the southern property line. Please indicate on the plat what these are for. Dedicated ROW?</p> <p>3.Between the 55.00' x 10.00' blocks and the 65.00' x 10.00' block there is a label indicating "Dedicated Right of Way" and dimension leaders, but no dimension is shown and the lines are too bold/wide to see any space between them to get an accurate measurement. Please include the measurement.</p> <p>4.The 40' Road Reservation for Division Ave needs to be changed on this plat and dedicated as a public right-of-way.</p> <p>5.A Drainage Report was not submitted. However, the County has been working with the design team for the new school regarding drainage and a report will be submitted with the Site Plan application submittal.</p> <p>Surveyors Comments:</p> <p>1.The northeast center line end of the 20' SEWER & WATER EASEMENT BOOK 690 -PAGE 525 is shown as 15.25' north of the point of intersection in the north boundary of LOT 1 BLOCK 1. Is the easterly portion of the north boundary of the 9' SEWER & DRAINAGE RESERVATION BOOK 1106 PAGE 1176 located on the boundary line of LOT 1 BLOCK 1 of this subdivision. Please clarify on the plat drawing.</p>	SCOTT.LARSON@LARAMIECOUNTY.WY.GOV
03/31/2026	Workflow	PUBLIC WORKS REVIEW	GENERAL	<p>1. All comments from the review engineer and surveyor shall be addressed and resolved appropriately.</p> <p>2. Future infrastructure upgrades may be required with this development. This could include but is not limited to access, right-of-way (road construction), re-vegetation and signage. Permit applications through Public Works will be required at the time of development.</p>	MOLLY.BENNETT@LARAMIECOUNTY.WY.GOV
04/01/2026	Application	PZ-26-00024	GENERAL	<p>WAPA has no conflict with this project.</p>	ROGERS@LARAMIECOUNTY.WY.GOV
04/03/2026	Application	PZ-26-00024	GENERAL	<p>Plat</p> <p>1.Dedicate 40 feet of right-of-way along the west side of this site for Division Ave.</p> <p>Traffic Study</p> <p>1. Executive Summary / Required Improvements (Pages 15) Clearly distinguish required mitigation measures from future monitoring items. The improvements required shall be clearly identified.</p> <p>2. Future Growth Assumptions (Pages 67) Provide the basis for long-range roadway assumptions and annual growth rates, including consistency with adopted County and MPO plans.</p> <p>3. Methodology (Pages 1114)</p>	CHRISTOPHER.YANEY@LARAMIECOUNTY.WY.GOV

Permit Notes

MPO HAS NO AUTHORITY IN THE COUNTY.

THESE COMMENTS ARE FOR THE SITE PLAN AND HAVE NO RELEVANCE TO THE PLAT.

Clarify the analysis software, HCM methodology, and school peak hour assumptions used in the LOS and queue analyses.

4. Existing Conditions / Safety (Pages 1520)
 Include observed operating speed data and expand the safety discussion to address school pickup/drop-off, pedestrian activity, and bus circulation.

5. Queueing / On-Site Circulation (Page 20)
 Provide a formal queue analysis for pickup and dismissal operations, including maximum queue length, storage provided, and spillback potential to Wallick Road.

6. On-Street Parking (Page 20)
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9. Signal Warrant Analysis (Pages 2428)
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10. Improvement Exhibit
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04/03/2026	Workflow	ENGINEERS REVIEW	GENERAL	2nd Review - The 40' Road Reservation for Division Ave shown to be vacated needs to be changed on this plat and dedicated as a public right-of-way to allow for the new/proposed Division Avenue to line up with the ROW for Division south of West Wallick Road and the intersection of Division and Wallick can be constructed properly in the future.	SCOTT.LARSON@LARAMIECOUNTY.WY.GOV
RIGHT OF WAY DEDICATED					
04/07/2026	Workflow	PUBLIC WORKS REVIEW	GENERAL	1. All comments from the review engineer and the Cheyenne MPO shall be addressed and resolved appropriately.	MOLLY.BENNETT@LARAMIECOUNTY.WY.GOV

RESOLUTION NO. _____

**A RESOLUTION TO APPROVE A VACATE OF LOTS 1 AND 2, BLOCK 1,
AFFLERBACH ADDITION, LARAMIE COUNTY, WYOMING, TO BE PLATTED AND
KNOWN AS
"AFFLERBACH ADDITION, 2ND FILING"**

WHEREAS, Wyoming State Statutes §18-5-201 to 18-5-208; §18-5-301 to 18-5-315 authorize Laramie County, in promoting the public health, safety, morals and general welfare of the county, to regulate the use of land through zoning in unincorporated Laramie County; and

WHEREAS, the Laramie County Board of Commissioners adopted the 2025 Laramie County Land Use Regulations; and

WHEREAS, the proposed modest plat is in accordance with section 4-2-103 of the 2025 Laramie County Land Use Regulations; and

WHEREAS, the application is in conformance section 2-4-109 with the P – Public zone district; and

WHEREAS, this resolution is the subdivision permit for Afflerbach Addition, 2nd Filing.

NOW THEREFORE BE IT RESOLVED BY THE GOVERNING BODY OF LARAMIE COUNTY, WYOMING, as follows:

The Laramie County Board of Commissioners finds that:

- a. This application is in conformance with section 4-2-103 of the 2025 Laramie County Land Use Regulations governing Modest Plats with lot line adjustments within recorded subdivisions.
- b. This application is in conformance with section 2-4-109 governing the P – Public zone district.

And the Board approves the Vacation of Lots 1 and 2, Block 1, Afflerbach Addition, and approve the Modest Plat for Afflerbach Addition, 2nd Filing with one condition.

1. **The Vacation of Lots 1 and 2, Block 1, Afflerbach Addition will become effective upon the recordation of Afflerbach Addition, 2nd Filing.**

PRESENTED, READ, AND ADOPTED THIS _____ DAY OF _____, 2026.

LARAMIE COUNTY BOARD OF COMMISSIONERS

Gunnar Malm, Chairman

ATTEST:

Debra K. Lee, Laramie County Clerk

Resolution reviewed and approved as to form:

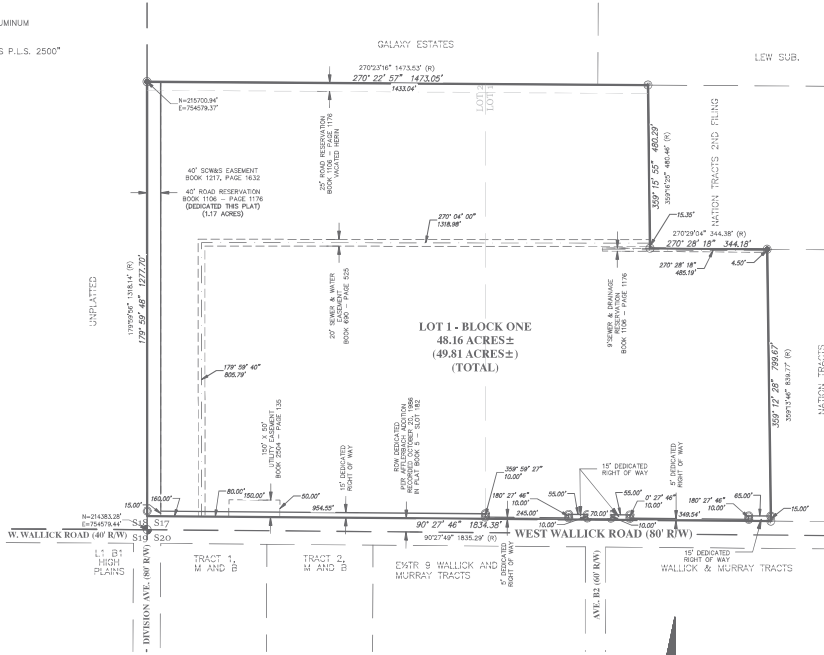


Laramie County Attorney's Office

WATER AND SEWER PROVIDED BY SOUTH CHEYENNE WATER AND SEWER DISTRICT FIRE PROTECTION TO BE PROVIDED BY FD-1
 THE SURFACE ESTATE OF THE LAND TO BE SUBDIVIDED IS SUBJECT TO FULL AND EFFECTIVE DEVELOPMENT OF THE MINERAL ESTATE

LEGEND

- SET 3/8" X 24" LONG REBAR WITH 1/2" ALUMINUM CAP STAMPED "SSS P.L.S. 5910"
- FOUND 1/2" ALUMINUM CAP STAMPED "SSS P.L.S. 2500"
- FOUND 2" BRASS CAP STAMPED LS 519
- FOUND 1" IRON PIPE
- (R) DENOTES RECORD DATA
- BOUNDARY LINE
- LOT LINE
- PROPERTY LINE
- - - EXISTING EASEMENT
- - - SECTION LINE



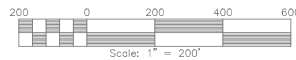
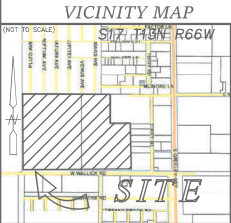
VACATION STATEMENT

IT IS THE INTENT OF THIS REPLAT TO VACATE LOT 1 AND LOT 2, BLOCK ONE, AFFLERBACH ADDITION, SITUATE IN A PORTION OF SECTION 17, T13N, R66W, OF THE 6TH P.M., LARAMIE COUNTY, WYOMING. EXISTING EASEMENTS TO REMAIN AS SHOWN.

NOTES

1. BASIS OF BEARINGS, WYOMING STATE PLANE, COORDINATES, EAST ZONE, NAD83-2011, US SURVEY FEET, GRID DISTANCES, COMBINATION FACTOR 0.99966220935.
2. ALL UNMONUMENTED TRACT CORNERS, ANGLE POINTS AND CURVE POINTS TO BE MONUMENTED WITH A 1/2" ALUMINUM CAP STAMPED "SSS PLS 5910" SET 3/8" X 24" REBAR.
3. NO PORTION OF THIS PLAT FALLS WITHIN A FEMA 100-YEAR SPHA PER F.I.R.M. PANEL #56021C1092F REV 04-14-2011.
4. CAPP - SUBJECT PARCEL DOES NOT FALL WITHIN THE CAPP.
5. NO PUBLIC MAINTENANCE OF INTERNAL ROADWAYS/ACCESS EASEMENTS.
6. 0.48 ACRES OF NEW DEDICATED RIGHT OF WAY THIS PLAT.

FILING RECORD



REVISED: 4/9/2026
 26149 - AFFLERBACH REPLAT.DWG

DEDICATION

KNOW ALL PERSONS BY THESE PRESENTS THAT: Laramie County School District No. 1, owner in fee simple of Lot 1 and Lot 2, Block 1, Afflerbach Addition, situate in a portion of Section 17, T13N, R66W, of the 6th P.M., Laramie County, Wyoming.

Have caused the same to be vacated, replatted and known as: AFFLERBACH ADDITION 2ND FILING, and do hereby declare the subdivision of said land as it appears on this plat, to be their free act and deed and in accordance with their desires and do furthermore dedicate the roads and easements for the purposes shown hereon.

Andy Knapp, Executive Director of Support Operations
 for Laramie County School District No. 1

OWNER ACKNOWLEDGEMENT

STATE OF WYOMING)
 COUNTY OF LARAMIE) SS
 The foregoing instrument was acknowledged before me this _____ day of _____, 2026, by Andy Knapp, Executive Director of Support Operations for Laramie County School District No. 1, for Laramie County School District No. 1.

Notary Public

APPROVALS

Approved by the Board of Laramie County Commissioners of Laramie County, Wyoming, this _____ day of _____, 2026.

Chairman _____ ATTEST: _____
 County Clerk

CERTIFICATE OF SURVEYOR

I, Jeffrey B. Jones, A Professional Land Surveyor in the State of Wyoming, for and on behalf of Steel Surveying Services, LLC, hereby state, to the best of my knowledge, information and belief, that this map was prepared from field notes taken during an actual survey made by me or under my direct supervision; and that this map correctly shows the results of said survey and that the monuments found or set are as shown.

AFFLERBACH ADDITION 2ND FILING

A MODEST REPLAT OF LOT 1 AND LOT 2, BLOCK 1, AFFLERBACH ADDITION, SITUATE IN A PORTION OF SECTION 17, T13N, R66W, OF THE 6TH P.M., LARAMIE COUNTY, WYOMING
 PREPARED FEBRUARY 2026



STEEL SURVEYING SERVICES, LLC
PROFESSIONAL LAND SURVEYORS
PLANNING & DEVELOPMENT SPECIALISTS
 1108 WEST 10th ST. CHEYENNE, WY. 82001 • (307) 694-7873
 756 GILCHRIST ST. WHEATLAND, WY. 82201 • (307) 328-9786
 www.SteelSurvey.com • info@SteelSurvey.com