

WEST CROW CREEK GREENWAY PLAN

EXECUTIVE SUMMARY

MARCH 2026



CHEYENNE
METROPOLITAN
PLANNING
ORGANIZATION

AVRES



PLAN INTRODUCTION

- Cheyenne has 47+ miles of Greenway since 1992; more planned as City grows.
- West Crow Creek segment:
MLK Jr. Park to Freedom Elementary.
- Includes updates to the asphalt existing Greenway in MLK Jr. Park
- Goals: community input, safety & accessibility, further completion of the 1992 plan.
- West Corw Creek segment is the only section of the 1992 Plan not yet built





CONSIDERATIONS

- Major crossings:
 - Missile Drive
 - BNSF railroad embankment
 - Westland Rd
 - I-25
 - Happy Jack Rd.
- Floodplain challenges: permitting & environmental impacts.
- I-25 corridor - WYDOT & FE Warren AFB coordination required.

STAKEHOLDER AND PUBLIC ENGAGEMENT

- Ongoing engagement: WYDOT, FE Warren AFB, Cheyenne Chamber of Commerce, Cheyenne MPO, City departments - Engineering, Public Works, BOPU
- Public meetings: May 2024 & July 2025
- Feedback documented in progress meetings and contributed to scoring of alternatives.

WEST CROW CREEK GREENWAY PLAN

3 4
CONCEPTS 3 AND 4: BNSF RR STRUCTURE
 THESE CONCEPTS WOULD PUT THE GREENWAY ADJACENT TO MISSILE DRIVE. THE GREENWAY PATH WOULD BE RESTRICTED TO 9'-6" UNDER THIS STRUCTURE.

1 2 3 4 5
ALL CONCEPTS: WESTLAND ROAD UNDERPASS
 GREENWAY TO GO UNDER EXISTING WESTLAND ROAD BRIDGE (OPTIONS 3 AND 4 WILL BE ON THE NORTH SIDE OF THE BRIDGE. OPTION 5 WILL BE ON THE SOUTH SIDE OF THE BRIDGE. PHOTOS SHOW THE NORTH SIDE OF THE BRIDGE)

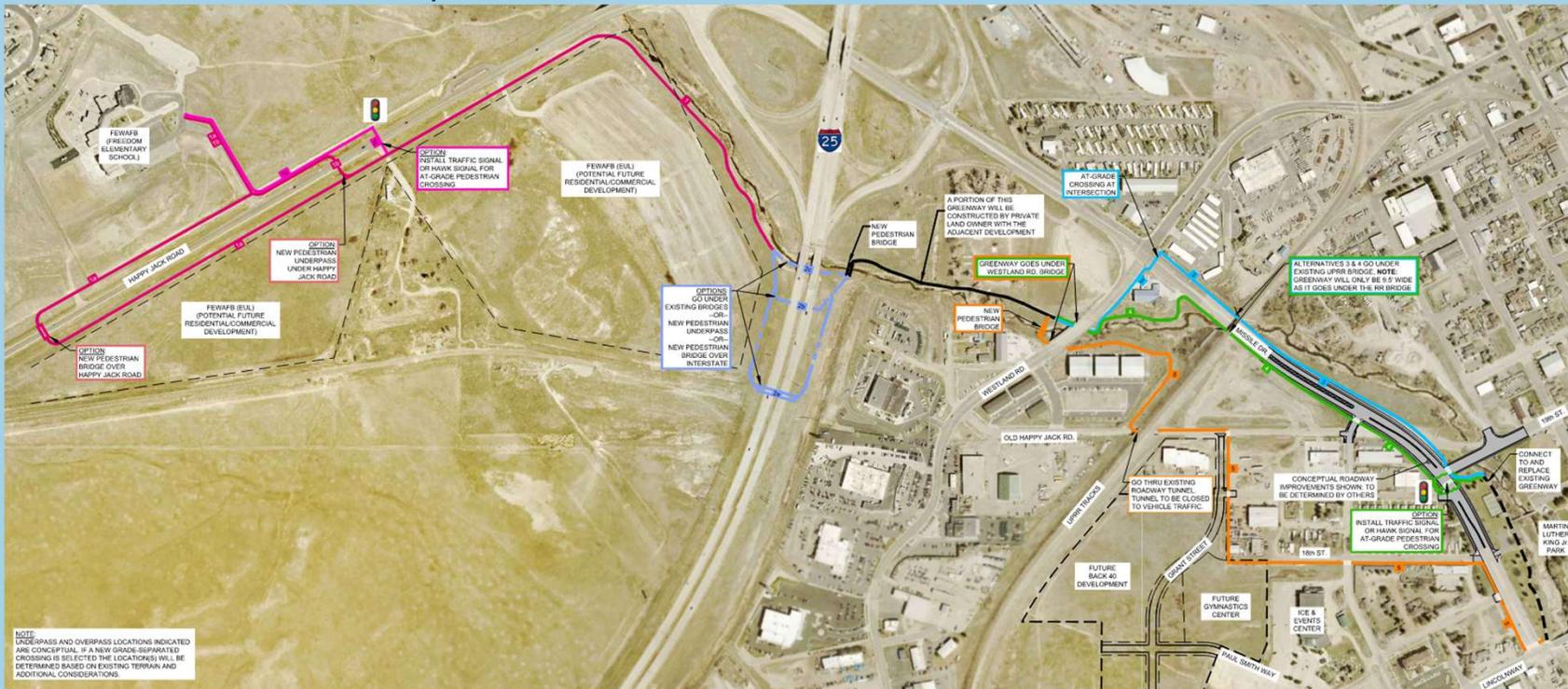
3 4 5
CONCEPT 5: OLD HAPPY JACK ROAD
 THIS CONCEPT WOULD ELIMINATE VEHICLES FROM USING THE OLD HAPPY JACK ROAD TUNNELS AND DEDICATE THIS SPACE TO THE GREENWAY.

2 3 4 5
MARTIN LUTHER KING JR. PARK
 THE EXISTING ASPHALT GREENWAY TRAIL THROUGH THE PARK WILL BE REPLACED WITH A 10' WIDE CONCRETE TRAIL AS PART OF THE FUTURE CONSTRUCTION PROJECT.

AVRES **CivilWorx**

EXPLORED ALTERNATIVES

- 10 initial alignment/crossing options narrowed to 5 based on stakeholder input.



EXPLORED ALTERNATIVES

- Stakeholder scoring based on an evaluation matrix including the following categories:

- Safety
- Security
- Flooding Risk
- Connectivity
- Relation to Greenway's purpose
- ROW & Utility Impacts
- Cost
- Environmental & Cultural Impacts



West Crow Creek (CC) Greenway Plan - Alternative Evaluation Matrix

#	Alternative Description	Evaluation Criteria										Total Score Max Score 33/30	
		Meets Purpose of Greenway	Safety		Operations			Impacts					
			Vehicular Conflict Points	Safe School Crossing	User Experience & Comfort	Connectivity & Access	Potential for Flooding	Construction Cost	Maintenance Cost	Right of Way	Utilities	Cultural / Archeological Impacts	
Happy Jack Road Crossing													
1a	Construct New Happy Jack Rd Overpass												
1b	Construct New Happy Jack Rd Underpass												
1c	Construct New At-Grade Traffic Signal/HAWK or Utilize Future Traffic Signal for EUL Land Development												
Interstate 25 (I-25) Crossing													
2a	Construct New I-25 Overpass Construct New Bridge to Cross Crow Creek												
2b	Construct New I-25 Underpass												

CONCLUSION

PREFERRED ALIGNMENT



CONCLUSION

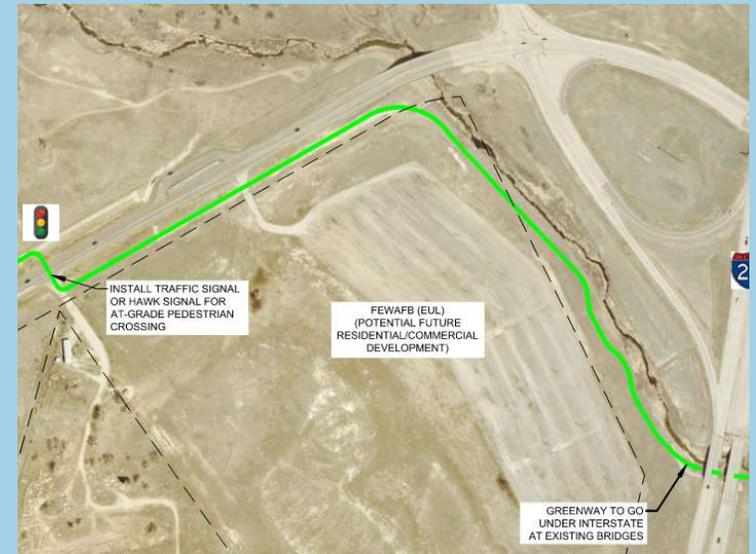
PREFERRED ALIGNMENT



Signalized at-grade crossing at Happy Jack Road (HAWK or signal).
Greenway inside EUL property.



West Bank alignment from Happy Jack to I-25; underpass at existing I-25 & Crow Creek bridge.



CONCLUSION

PREFERRED ALIGNMENT



East Bank alignment from I-25 to Westland Rd (with low-water crossing)



Underpass at existing Westland Road & Crow Creek bridge



CONCLUSION

PREFERRED ALIGNMENT



Alignment leaves creek, parallels Missile Drive; crosses under BNSF railroad



Connects to MLK Jr. Park via Lincolnway intersection



CONCLUSION

PREFERRED ALIGNMENT



Connections to Back 40 Subdivision, Ice & Events Center, and Gymnastics Center.



Future links to Commuter Rail Development as growth occurs.



CONCLUSION

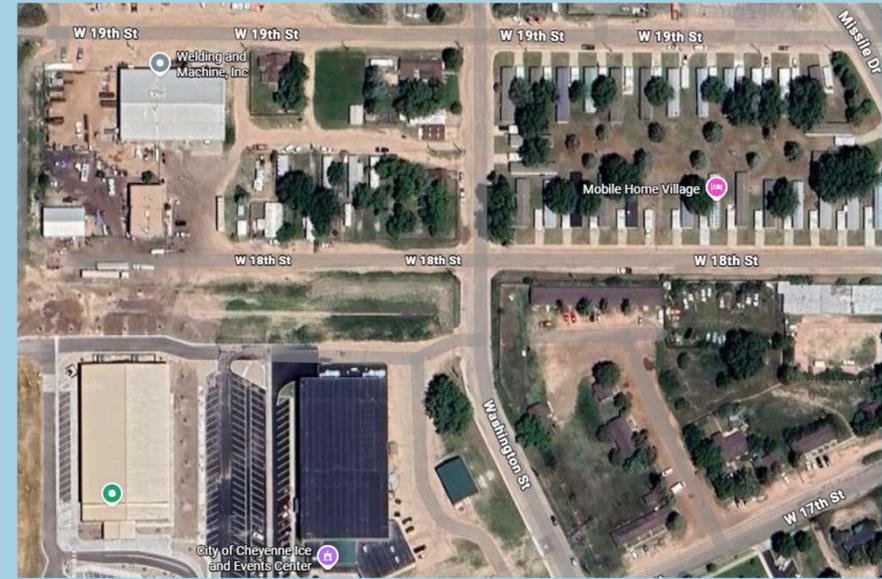
PREFERRED ALIGNMENT



Back 40 Subdivision, Ice & Events Center, Gymnastics Center.



Future links to Commuter Rail Development as growth occurs.



PROGRAM COSTS AND FUNDING STRATEGIES

- Estimated cost: \$3.6M (2025 dollars.)
- Up to 30% could shift to private development.
- Project segments should be completed with related and adjacent city projects
- Funding:
 - city budget,
 - state/federal grants (e.g., RTP)
 - private development



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NEXT STEPS

- Continued Stakeholder engagement & committee reviews.
- Implementation likely piecemeal; depends on funding & partnerships
- Make Greenway elements priority for adjacent projects & development.
- Future Greenway projects will follow standard design processes including environmental and stakeholder due diligence.

THANK YOU

Nathan Silberhorn
Senior Project Manager
970.223.5556

SilberhornN@AyresAssociates.com



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